

Final Agenda

**PLANNING COMMISSION REGULAR DOCKET
TUESDAY, August 11, 2015 – 5:30 P.M.
CITY COUNCIL CHAMBERS**

- I. PLANNING COMMISSION GATHERING -- 4:30 P.M. (Held in the NDS Conference Room)**
Commissioners gather to communicate with staff. (4:30-5:30 P.M.)

- II. REGULAR MEETING -- 5:30 P.M.**
 - A. COMMISSIONERS' REPORTS**
 - B. UNIVERSITY REPORT**
 - C. CHAIR'S REPORT**
 - D. DEPARTMENT OF NDS**
 - E. MATTERS TO BE PRESENTED BY THE PUBLIC NOT ON THE FORMAL AGENDA**
 - F. CONSENT AGENDA**
(Items removed from the consent agenda will be considered at the end of the regular agenda)
 1. Minutes – July 14 2015 – Pre-Meeting
 2. Minutes – July 14, 2015 – Regular Meeting
 3. Minutes – July 28, 2015 – Work Session
 4. Preliminary Site Plan – 1725 Jefferson Park Avenue
 - G. ENTRANCE CORRIDOR REVIEW**
 1. 1130 East High Street
 - H. WORK SESSION (NDS CONFERENCE ROOM)**
 1. West Main Street Zoning
 2. Public Comment
 3. Development Review Process
 4. Public Comment

Date and Time	Type	Items
Tuesday, August 25, 2015 – 5PM	Work session	Small Area Plans Capital Improvement Program
Wednesday, September 9, 2015 – 4:30 PM	Pre- Meeting	
Wednesday, September 9, 2015 – 5:30 PM	Regular Meeting	Market Plaza SUP Amendment Willoughby Site Plan

Anticipated Items on Future Agendas

- Harmony Ridge Subdivision Plat
- Spot Blight – 1810 Yorktown Drive
- Grove Street PUD Site Plan

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PLEASE NOTE: THIS AGENDA IS SUBJECT TO CHANGE PRIOR TO THE MEETING.

MINUTES
PLANNING COMMISSION REGULAR MEETING
Tuesday, July 14, 2015

I. PLANNING COMMISSION PRE-MEETING (Beginning at 4:30 p.m.)

Location: NDS Conference Room, Charlottesville City Hall, 2nd Floor

Members Present: Chairman Dan Rosensweig; Commissioners Lisa Green, Kurt Keesecker, Genevieve Keller, Jody Lahendro, and UVA representative Bill Palmer

Call to Order: the meeting was called to order by Chair Rosensweig at 5:00 p.m.

Commissioner Lahendro noted a concern with the minutes of the Joint Planning Commission meeting, that Moore's Creek was incorrectly noted.

Chairman Rosensweig asked if there were any concerns about the consent agenda. There were none.

Chairman Rosensweig asked for any questions regarding the Longwood PUD Amendment.

Commissioner Keller asked why the application was returning the Commission.

Matt Alfele, Planner explained why the City Council had referred the item back to the Commission.

Chairman Rosensweig asked if there were any questions about the transient lodging proposal.

Commissioner Keller asked about how the draft deals with three persons in a unit

Mr. Alfele stated that that item is up for discussion.

Commissioner Keller asked what changes were being made to the bed and breakfast definitions.

Mr. Alfele answered and said he would explain in Council Chambers.

Chairman Rosensweig asked if homestays were by-right.

Read Brodhead, Zoning Administrator said that they were.

Lisa Robertson, Chief Deputy City Attorney said that the City has been requiring Provisional Use Permits (PUP) for them.

Chairman Rosensweig asked why the City was requiring PUPs.

Mr. Brodhead stated that PUPs are used for tracking purposes.

Miss Creasy, Interim Director of Neighborhood Development Services said that home occupations also require PUPs.

Commissioner Keller asked if any other home occupations require the same level of requirements that the City would require for transient lodging.

Mr. Brodhead said that there were not.

Commissioner Green noted that the City does not, but other localities do.

Ms. Robertson said that the City does have transient lodging requirements.

Commissioner Keller asked about the sign regulations for transient lodging facilities.

Ms. Creasy stated that it was already dealt with in other areas of the code.

Commissioner Keller noted that they could require a smaller sign.

Mr. Brodhead said that the code can be written that way, and that most home occupations do not have the need to advertise.

Commissioner asked if the Commission could loosen or strengthen the regulations.

Chairman Rosensweig asked the Commission how they envisioned the process of reviewing the ordinance.

Commissioner Keller stated that she had a whole list of items. She felt they could go down the list item by item, or commissioner by commissioner.

Commissioner Green said she preferred to go down the list issue by issue.

Commissioner Keller stated that she saw no reason for the deliberation to be expedient. She said that minority opinions can change other Commissioners' minds, and that there was a lot of confusion around this proposal.

Commissioner Green said that some of the confusion is from people not listening or not understanding the proposal.

Chairman Rosensweig asked if there were any questions about 550 East Water Street.

Commissioner Green asked if there was a definition of density when used on the context of the Comprehensive Plan.

Brian Haluska, Principal Planner said that he preferred the term “intensity” because density is often used to refer to residential density, and not the overall impact of the building. He noted that there are buildings that make a large impact on areas that have a residential density of 0.

Commissioner Green asked how the Comprehensive Plan goals apply to the application.

Ms. Robertson said that most Comprehensive Plan goals are open to some interpretation by the Commission.

Chairman Rosensweig asked if there were any questions on 1725 JPA.

Commissioner Keller asked if Council had approved the design.

Mary Joy Scala, Design Planner read the condition from the Special Use Permit, and noted that the condition did require Entrance Corridor Review.

Commissioner Keller asked if the City Council should be putting conditions on items that come back to the Planning Commission for design review later.

Ms. Robertson said that Council does have the authority to put conditions on Special Use Permits.

Commissioner Keller stated that she did not feel she could be impartial in the review of this item, and that she was going to leave the room when it was called.

Vice-Chairman Keesecker said that the City used to let SUPs be vague, but now the City was placing conditions regarding the plan as submitted with the SUP.

Commissioner Keller said that she avoided talking about the design of the building when the SUP was before the Commission because she understood it would be returning to them later.

Ms. Robertson said that the Commission needs to be considering the Entrance Corridor guidelines when considering an SUP.

Ms. Scala said that the EC reviews the building according to the guidelines, and if the building meets those guidelines it must be approved.

Adjournment: At 5:27 p.m. the Chair adjourned the meeting in order to reconvene in City Council Chambers at 5:30 to continue with the Commission’s regular monthly agenda.

II. ADMINISTRATIVE AGENDA (Beginning at 5:30 p.m.)

Location: City Council Chambers, Charlottesville City Hall, 2nd Floor

Members Present: Chair Rosensweig; Commissioners Lisa Green, Kurt Keesecker, Genevieve Keller, Jody Lahendro, and UVA representative Bill Palmer

Call to Order: the meeting was called to order by Chair Rosensweig at 5:30 p.m.

A. Commissioner's Reports:

Commissioner Lahendro – reported he was unable to attend the Park and Recreation Advisory Board meeting in June because of conflict with another Board meeting. He did attend the Tree Commission meeting on June 24th. Much of the discussion revolved around urban tree canopy assessment that the City has contracted for. This is the first time it has been done since 2009. It will start in September and be finished in December. It is funded by a grant and already the tree planting committee with the Tree Commission is starting to strategize on how they will be able to use this information for advocating the planting of more tree canopy. There was a presentation by the Tree Commission to the Place Design Task Force on the health problems with some trees on the downtown mall and then the Tree Commission made some recommendations for 400 West High Street, the paper street that is there and is being shared with the BAR where a decision is to be made about that space.

Commissioner Keller – reported the TJPDC did not meet in July. The PLACE Task Force did have a brief meeting and the main focus was discussion about the lighting study that is being done with the City under contract and several people went around with the consultants. The Task Force requested some additional information on this. The Planning Commission will want to follow the process because it relates to things we have talked about such as revision to the Standards and Design Manual, Streets That Work, and the Code Audit. She attended the City Council meeting last week where the Council considered the William Taylor Plaza and the Council did not take any action on that and is something they will be taken up at a future meeting. She said it became evident to her that we need to be so careful when we craft PUD or SUP approvals when they come back years later because it is really hard to determine what the intent was and that was a project that was approved without a matrix at the time. She said that was not so many years ago but some of the players are the same, but it is really difficult for people to revisit that. We need to work together to make sure we are specific in our language and site the actual drawings and supporting materials that we believe are critical in our decision so if somebody has to look at it in 8 months, 5 years or decades that there are tangible pieces of evidence to see what was intended that they can go to. In the future, she feels we should be cognizant of when we look at these requests that we are leaving a really good record for the future.

Commissioner Dowell - absent

Commissioner Keesecker – reported the BAR met June 16th and reviewed a number of projects. The projects of interest to the planning commissioners were 550 E. Water Street which we have the summary in our staff report. The other two are applications for 500 Court Square, the Monticello Hotel or the Court Square Tavern building a discussion about screening of changes to the cellular infrastructure that is on top of the building that we can see from far away and we've taken a look at people trying to get something off the penthouse and bring them down to the deck level. Essentially what the BAR asked for was a coordinated building managed master plan, to bring a cohesive screening to the top of the building which is very prominent. The other project was a proposal for a small cafe on West Main Street directly across from the Flats where the

Standard was going to be, at the current Republic Plaza, where there is a small space that has some existing trees and a bus stop. They want to provide outdoor seating without removing any trees with a covered top and then renovate that part of the building and possibly open it up as a restaurant and bring some activity to the street. He said there were some nice images and it was well done. He said the Standard is still on going, the SUP is going to stay in place. It is not going to be built not as a tent structure or masonry but it was basically precursor to the Standard being built. It would not change what we've seen for the Standard. It is a by right use.

Commissioner Santoski – absent

Commissioner Green – reported that the July meeting of C-Tech was cancelled being a holiday week and lack of participation, the meeting will resume in September.

B. University Report—Bill Palmer reported that it is summer construction season over at the University. Some of the bigger projects are the Rotunda renovation which is ongoing such as the roof getting painted white, McCormick Road is completely dug up to replace utility tunnels, and near the Alderman Dorm area, the newest Dorm, Givens Hall is just about finished which is the final new dorm built across from the aquatic fitness center. Lastly, there is a large pedestrian bridge between the Alderman Dorms and Gooch-Dillard housing area which use to be upper classmen and now is first year housing which will help that community feel a little more integrated with the other first year dorms.

C. Chair's Report—Chair Rosensweig reported the Housing Advisory Committee met in sub-committees last month. A comprehensive housing survey has been convened. This was a group of non-profit leaders primarily of housing organizations and agencies who volunteered to help administer the survey to target low-income populations. Previously the scope of work for housing preference had been only to target work force and didn't really have any plans to reach out to Non-profit, low income residents to find out what potential barriers to appropriate housing might be out there. Volunteers are planning to be available to conduct surveys at the annual Westhaven Days on August 1st. The Rivanna River Committee met on June 23rd to discuss the next steps in the process of forming a plan to bring back the river as a central cultural future of the community. TJPDC Executive Director Chip Broyles gave us and the Albemarle Planning Commission an update of how that process is going at our meeting on the 23rd. But among other recommendations the group is going to recommend to Council and to the Board of Supervisors some smaller planning studies at various points along the river, as well as some funding mechanism to support some of the nonprofits to help keep the river clean.

This morning the Code Audit and Streets That Work steering committee met and he is happy to report that the team is making tremendous progress, thanks to the professional stewardship of planners Heather Poole and Amanda Poncy. The group met again with representatives from Toole Design Group and talked about a set of design guidelines and an implementation plan. Today the focus was on an existing conditions report that Toole Design Group has drafted. There are three community out-reach opportunities planned: July 25th, 10:30 – 2:30 as part of the African-American arts Festival Washington Park representatives of the city, the Toole Design Group, and this committee will be there talking about some of the findings and getting some perspectives about the Streets That Work. Also at Westhaven Days on August 1st 10:00 to 1:00 on

Hardy Drive, the same will take place. An open house on September 15th and 16th for the general public, the time and place will be announced. There are two things of special interest is that on both the African-American Arts Festival and the Westhaven Days, community driven street murals are going to be done in chalk, originally done as tested projects at Westhaven. The search for the Neighborhood Development Services director is still ongoing. Currently the city manager, Maurice Jones, is conducting reference checks for the top candidates. He complimented to Maurice Jones and staff for organizing a professional and fair process and looking forward to meeting the new Director of Neighborhood Development Services.

D. NDS Department Report: given by Brian Haluska, Principal Planner reported the next work session July 28th and the items on the agenda is the West Main Streetscape, Development Review Policy. Today we recognized out traffic engineer Donovan Branche who will be leaving the city and we gave her a surprise farewell party this afternoon.

E. Matters from the Public Comment

F. CONSENT AGENDA

(Items removed from the consent agenda will be considered at the end of the regular agenda)

1. Minutes – June 9, 2015 – Pre-Meeting
2. Minutes – June 9, 2015 – Regular Meeting
3. Minutes – June 23, 2015 – Work Session
4. Site Plans and Subdivisions Approved Administratively
5. Subdivision Plat – Naylor Street

Motion by: Commissioner Green

Seconded: Commissioner Lahendro

VOTE: 5-0

“Aye”: Commissioners Green, Keesecker, Keller, Lahendro, Rosensweig,

“Nay”: None

Abstentions: None

Disqualifications: None

Planning Commission Meeting Resumed at 6:00 p.m. when a quorum of City Councillors were present.

G. JOINT PUBLIC HEARINGS

1. **ZM15-00001 – Longwood Drive PUD Amendment** – Richard Spurzem of Neighborhood Properties LLC, has submitted a PUD amendment to add four (4) attached residential units to the existing Longwood Drive PUD development. The additional units will be located on the southwest corner of Harris Road and Longwood Drive. The original PUD was approved July 20, 2009. Additions to the approved proposal include expansion of the existing PUD by 0.20 acres, constructing four (4) attached residential units, additional parking, and adding 2,705 square feet of open space. The property is further

identified on City Real Property Tax Map 21A Parcel 104, having frontage on Harris Road and Longwood Drive. The site is zoned R-2 and the total project area is 8,712 square feet or approximately 0.02 acres.

Matt Alfele gave the report on the Longwood Drive PUD Amendment. The changes submitted after the May 12, 2015 public hearing were outlined below:

- The applicant has changed the development from five (5) townhomes to two (2) duplexes for a total of four (4) new units.
- The duplex facing Harris Road is two (2) stories to keep in context with surrounding properties.
- The duplex facing Harris Road would no longer have garage parking.
- The duplex facing Harris Road would have front porches to increase street life and add to the surrounding neighborhood.
- The duplex at the south end of the development has been moved to allow more separation with the existing development. That separation has increased from fifteen (15) feet to twenty-nine (29) feet. This change is reflected in the development plan and included as an additional proffer.
- Open Space has increased from One thousand five hundred sixty-five (1,565) square feet to Two thousand seven hundred and five (2,705) square feet.
- Parking has increased from seven (7) driveways to eight (8) driveways.
- Proffer statement from the previous submittal has not changed with the exception of a new proffer requiring a 29 foot setback from the southern property line.

Mr. Alfele said that staff finds that incorporating two (2) duplexes for a total of four (4) units into the existing Longwood PUD complies with many of the goals laid out in the Comprehensive Plan, but some concerns remain.

He said the principal concern staff has is the fulfillment and documentation of the 2009 proffers. The applicant has stated that three (3) of the proffers have been satisfied, but staff would like more detailed documentation on how that was determined. Staff would also like more clarification on how proffer # (5) will be fulfilled. The addition of proffer # (6) and proffer # (9) are very much welcomed by the City. Staff recommends proffer # (9) be clarified to address just this one area of the development. As written it could be applied to other areas of the development. Staff has some reservation about the addition of a wide curb cut so close to Longwood Drive. This could be problematic for pedestrians and school children as it would create an additional obstacle to cross. The fact that the applicant is asking to expand the Longwood Drive PUD before the original development has been built-out is also of concern to staff and the surrounding neighborhood. It is the understanding of staff that the 15% affordable units have not been built yet and are planned for the southern end of Longwood Drive. The introduction of a phasing plan with timetables would be helpful so the City and surrounding neighborhoods fully understand the timeframe of Longwood Drive PUD.

He said staff believes that the applicant has incorporated feedback from the Planning Commission and adjacent property owners into the most recent submission. The units facing Harris Road are more appropriate to the neighborhood and have the potential to add street life to Harris Road. The units facing Longwood Drive now provide more separation from the existing development.

Staff finds the Longwood PUD amendment complies with the goals of the Comprehensive Plan and recommends approval.

Mike Myers, Dominion Engineering, gave a brief history of the project and the changes made.

Richard Spurzem, Applicant stated that the desire for off street parking came from the residents in the development who would like to see a dedicated spot for the three bedroom units, since the residents usually have at least 2 cars, and they want enough parking for two spots per units. The neighbors commented that they want any new units to have off street parking. The housing price in most of the townhouses already sold has met the affordable price. They feel the provision has been met. They met the formula in the original PUD.

Council Kristin Szakos stated that the housing ordinance requires that affordable units be maintained as affordable for a certain time period.

Mr. Spurzem said the requirements for Longwood were outlined in the proffers. He said the City policy was passed after the original development was approved.

Mr. Alfele said that the affordable unit proffer says that 15% of the dwelling units those document is something staff has requested in the past.

Council Szakos said they have not fulfilled the terms of the proffers and that until we have in documentation the evidence to fulfill this proffer, this proffer has not been met.

Chairman Rosensweig asked about the possibility of eliminating 2 of the 4 parking spaces.

Mr. Haluska said that as long as everyone would be satisfied, and the traffic engineer agreed to the design.

Commissioner Keller said she wanted to make it safe for the pedestrians.

Open Public Hearing

David Hennegan, 101 Longwood Drive, Lot 116, said he appreciates the conditions that have been met. Support the spirit of the changes and that the additional parking spaces are necessary.

Lisa Pisani 114 Longwood Drive, said she was not opposed to anything being built, more of the sunlight to be block, and deceasing property value. If this new development is allowed to proceed, it will be too close to my own home and all we want is to keep it nice and quiet and peaceful the way it is.

Rebecca Quinn – Asked about the paving methods that will be used, and do they satisfy the storm water runoff regulations.

Closed Public Hearing

Commissioner Keller asked about the pedestrian conditions around the site.

Mr. Alfele said there is a crossing guard at the school. He said the City traffic engineers were not concerned about this, but that something could be built to address the concerns.

Commissioner Green said that we are trying to create walking in the city. She said she does not like the open spaces on the PUD because they may become potential building lots. She said the Commission rejected the changes before. She said she was more in favor of removing the on-site parking, and adding on-street parking.

Commissioner Rosensweig asked Mr. Spurzem if could say whether there was parking on both sides of Longwood Drive.

Mr. Spurzem said that the S-curve on Longwood seemed to make people less likely to park on the S curve. He said a curb extension would bring new on street parking for this community

Commissioner Keller said her concerns for pedestrian safety and the proffers have not been met.

Commissioner Keesecker said the concerns about curb extensions could be discussed with the site plan.

Mr. Alfele said the plan had been reviewed by the traffic engineers, and they felt it more appropriate to have the large curb cut.

Commissioner Green move to recommend approval of this application to amend the development plan for the Longwood Drive Planned Unit Development with amended proffers, on the basis that the eliminate off street in favor of on street parking on subject to traffic engineer proposal would serve the interests of the general public welfare and good zoning practice, seconded by Commissioner Lahendro, motion passed 5-0.

2. **ZT14-00011 – Transient Lodging Facilities** - A proposed zoning text amendment, to amend and reordain § 34-1200 and § 34-1172 of the Zoning Ordinance of the City of Charlottesville, to provide a definition of “transient occupancy”, and to provide amended regulations under which a residential dwelling unit may be used as a transient occupancy facility, within all zoning district classifications where Home Occupations are allowed.

Mr. Alfele presented the staff report.

Commissioner Lahendro asked what legal backing the City had if the people do not comply.

Mr. Brodhead said 95% if not more, try to do right thing, but there are some bad actors that slip through the cracks.

Ms. Robertson said the ordinance has definition for residential occupancy, transit occupancy added onto a residential use, you want to limit the number guest or the number of room otherwise you have a definition to transient occupancy. You need to spell it out in this ordinance.

Commissioner Green asked if the police could weigh in on this.

Mr. Brodhead said that NDS staff would use the police to help solve these problems.

Mayor Huja asked if an owner can rent a house just once or twice a year.

Mr. Brodhead said no they would not be able to be operating anymore. They would not be able to do it.

Commissioner Green said she does not think the SUP is the best thing for this, because it changes the integrity of the neighborhood.

Commissioner Jody Lahendro said he just did not want it to be going on the majority of days in the year.

Open Public Hearing

Travis Wilburn said former and current city officials have told him his businesses, which manages approximately 60 properties for short- and long-term rentals, are legal. After paying \$300,000 in taxes in recent years and being told in 2014 by former Neighborhood Development Services Director Jim Tolbert that Stay Charlottesville was “a good example of how transient lodging facilities should operate,” the business could be in danger of becoming an illegal operation. Mr. Wilburn stated that this is a code for home occupations, not short-term rentals. If short-term rentals are going to be regulated, they need to be regulated individually and not as a home occupation.

Mark Kavit, said he is concerned about home turned into to AirBnB. He said he had been approached about used his property for Air BnB operation. Some enforcement regulation, at least regulations some type of way to go about that. He said that right now he knew of three properties in North Downtown used for short term rental. Some enforcement regulation should be done in these. The structure should be a primary home, they have sleep there certain times a year.

Jean Hyatt 1534 Rugby Ave., said she is concerned about the use and urged that it be required them to be owner occupied in R1 neighborhood to have a small unit on the property. Do not permit transient lodging in R3 into long term residents.

Bill Chapman, Stay Charlottesville co-founder and member of the city’s Board of Zoning Appeals, said he wouldn’t have invested in the business five years ago if he didn’t believe it would be legal.

Joyce Guest Houses Arlington Blvd don’t understand the second home rentals, a lot of growth however that what a guest rental is. They use it part of the time. The majority are not second

owners, and came to me for a service. 2, there is a really strong need in this area for accommodations other than a hotel room to accommodate people for a week or two weeks long. We are tourist town. We are a Big tourist town. Tourism promotes tourist to come and stay longer. Those are her main issues. Signs she doesn't agree with. There is nothing in here for people who are leaving their homes for graduation weekend.

Closed the Public Hearing

Commissioner Green said this is a great start for this ordinance. Still not of a mind set to do the second home. Relationships with people all over the world. She would be sad that there would be a house purchased just for this reason. She said we could start with an ordinance and later we could always tweak it.

Commissioner Lahendro said his concern is about the protection of the residential community. This is like a financial temptation to start to damage residential communities. How many times to be rented to visitors before it starts to erode to connection to that community through these services.

Commissioner Keesecker said staff has been responsive to the conversation he's hearing, he has concerns about this provision may apply to the non-primary use from time to time, reservation about the definition about home occupied

Chairman Rosensweig said he thinks the thing most at stake is the character of the neighborhoods and it has to do with the density of this use in a given area. He also stated that there has to be a tipping point where it will no longer feel like a residential neighborhood.

Mr. Santoski arrived at 8:00 pm

Based on a finding that the proposed zoning text amendments will serve the public necessity, convenience, general welfare, or good zoning practice, Commissioner Keller moved to recommend approval of a zoning text amendment as proposed with the following changes:

- The use will be permitted by a Provisional Use Permit
- The owner of the property is not required to be onsite
- Allowable in low density residential zoning and multi-family condominiums
- No notification of adjacent owners shall be required.
- No posted evacuation plan is required
- A permanent owner should occupy the property
- No one unit restriction per Tax Map Parcel
- A responsible party shall be kept on record that can be reach at all hours.
- Yes to the revocation clause
- No limit on number of days per year
- No more than 6 adults, per visit, per parcel
- Smoke alarm and fire extinguishers required
- Addition of word "Overnight" in homestay definition guests
- No signage

to Section 34-1200 and 34-1172 of the Zoning Ordinance, to allow Transient Occupancy in residential dwellings (under the term “Homestay”) with a Provisional Use Permit in every zoning district where Home Occupation is allowed to add a limited to six adult guest per parcel, (Council consider some time limit)

Seconded by Commissioner Green, 4-2 motion passes. (Commissioners Keesecker and Rosensweig voting no)

- 3. SP15-00002 – 550 East Water Street** – Core Real Estate and Development has submitted a Special Use Permit application to increase height from 70 feet to 101 feet. The property is further identified on City Real Property Tax Map 53, Parcel 162.3 with road frontage on East Water Street. The site is zoned Water Street Corridor with Architectural Design Control District Overlay and Parking Modified Zone Overlay. The parcel is approximately 0.28 acres or 12,200 square feet. The Land Use Plan calls for Mixed Use.

Mr. Haluska presented the staff report.

Mr. Andrew Baldwin, developer, explained that the current plan is to break the building into two components. The section next to the former C&O train station would be constructed to a maximum height of 40 feet, while the section next to the King Building would rise to 101 feet.

Mr. Robert Nichols, architect, said the intention is to displace building mass and the interior volume of the building to make a different composition to make a better experience on the street. The project would feature three stories of office space on the quarter-acre lot with a single residential unit on each of the remaining six floors.

Commissioner Rosensweig said he could have supported the additional height, but the impact on Water Street would be too great. He added he liked the concept of splitting the building into tall and short sections, but 101 feet was too high.

Open Public Hearing

Mark Kavitt – 400 Altamont Street, said he is not against the tall story building, but it should not have nine stories. The project plan for office space and condos units costing 2 million dollars. He noted that there are parking problems for the area as well as problems with loading.

Morgan Butler, Southern Environmental Law Center, said this is out of scale for the location. He said his organization is not opposed to this particular site, between 2 historical buildings, but that the proposal would triple the height of these buildings. We don't see that as a reason tries to do too much for this site. Any by right will need to be reviewed by the BAR or place conditions on the By right. Trying to do too much for this building.

Samuel Hellmann, a resident of the Holsinger, located across the street from the proposed site, said the proposal drastically overburdens the triangle-shaped property. He said the worst thing you can do in a street is put a tall building on the south side, and have it shield the sun and will

completely be in the sight line of almost every place on the Downtown Mall. Mr. Hellman questioned whether granting the permit was worth it to the city, given that only six residential units would be created. He hopes he gets the contract and be able to build something that pleases the neighborhood. Why is there a tower, 6 luxury apartments and some parking. This is better it is the lesser of two evils, he considers and over burdens. More modest but does make water street just a canyon.

John Lawrence 213 West Main Street, said he has been a business owner since 1993 and is familiar the developer and the project. The irregularity in the downtown area is something that makes it interesting and makes Charlottesville progressive and forward-thinking. He said in Virginia we like how things used to be in the old days, but he loved how Charlottesville over the last few decades has really thoughtfully developed itself.

Tim Michel said this is really too much. It's too dense and impacts us too high, and we'll be in shadow for a good bit of the day.

Neil Sansovich said he is in favor of it and thinks what this makes the downtown exciting is the juxtaposition between new architecture [and old architecture] from good architects that can do something different that makes vibrancy that is so important.

Patty Myatt commented if Charlottesville loses its distinctive character, people will stop coming here because they don't want to come to just another version of Northern Virginia or Virginia Beach. We will lose our drawing power. Please do not approve this tall building.

Emilie Johnson 112 Fifth Street S.E said the proposal scale and massing the heights, setback and step back, located on the Southside, all have an open space. There are site lines from every direction. The water street elevator show discrepancies show the relationship toward the train station and the King Building. Shares concerns with loading and traffic, especially since Water is the main E/W quite a few concern, landscaping will make it a com, the size of the building, she thinks the street frontage street wall a concern, is actually very very lively, not technically development, the building scale, there are similarly, the new water house, the Omni, all are very large, the north of the building, the parking lot, Omni parking to the North. This structure casting shadows over her apartment. Lack of a loading zone.

David Myatt said he lives at the Holsinger, adverse impact on the neighborhood. Note that at BAR it has been said the commercial use is light duty transient office space, if that the case the applicant should be held to that commitment. It should be preserved. Street closure could be extreme. Heavy volume of traffic. At this location the street is not wide enough for two way traffic.

Lisa Hogan said she supports the argument raise 1. Waterhouse unoccupied, 455 South same solution the requirement of these properties, garage that was referenced, pleased that it is there. Proposed By right building, even that structure is longer and lower throughout the area and charming. Conducive to the lower class housing.

Close the Public Hearing

Mr. Nichols said the site is part of the downtown core and should reflect the city's growing urban character. We are clearly participating in the Fifth and Water Street intersection, and that's where we've put the bulk of occupancy.

Mr. Baldwin said the project is in the spirit of the Comprehensive Plan. Revitalization of dead areas within the city of Charlottesville is what we need to focus on and provide in a project of this scale. He added that he would create pocket parks on either side of the development and install wider sidewalks than currently exist.

Mr. Baldwin said he was willing to defer his vote to return with a smaller building.

Mr. Santoski move to recommend denial of this application for a special use permit, Seconded by Ms. Keller, 4-2, motion passes. Commissioner Keesecker and Commissioner Rosensweig voted against the motion.

Ms. Keller left the meeting at 11:25 pm

H. ENTRANCE CORRIDOR REVIEW

5. 1725 Jefferson Park Avenue

The applicant is requesting approval of a certificate of appropriateness to construct a six-story apartment building with 19 units, and garage parking for 32 cars and 20 bicycles. The site abuts Jefferson Park Avenue and Montebello Circle.

After a brief discussion by commissioners:

Commissioner Keesecker moved to approve the Entrance Corridor Certificate of Appropriateness application for the new apartment building at Jefferson Park Avenue and Montebello Circle, with the following conditions: The applicant will work with Ms. Scala appropriate stone for Landscaping, seconded Commissioner Green, 5-0 motion passes.

Adjournment 11:40 pm

MINUTES
PLANNING COMMISSION WORK SESSION
Tuesday, July 28, 2015
5:00 – 7:00 p.m.

I. PLANNING COMMISSION WORK SESSION

Location: NDS Conference Room, Charlottesville City Hall, 2nd Floor

Members Present: Chair Rosensweig; Commissioners Kurt Keesecker, Genevieve Keller, Jody Lahendro, and John Santoski, Lisa Green; UVA representative Bill Palmer, Carrie Rainey, Missy Creasy, Amanda Poncy, Brian Haluska, Lisa Robertson

Call to Order: by Chairman Rosensweig at 5:00 p.m.

1. West Main Street Streetscape Plan :

Carrie Rainey, Urban Designer provided the staff summary of the project:

In the face of several looming construction projects, the council voted in February 2013 to spend up to \$350,000 to hire a firm to develop a concept for the streetscape. The city recently hired Alexandria-based Rhodeside & Harwell to update earlier plans. The Rhodeside & Harwell plan would provide 9-foot sidewalks and wider bike lanes. As many as 33 on-street parking spaces would be removed. The concept also includes the removal of a dedicated right-turn lane from West Main to Ridge Street. The total cost estimate to implement all of the recommendations in the Rhodeside & Harwell study is \$30 million. A large portion of that would be to place utility lines underground. The future of the plan was called into question in January when Mayor Satyendra Huja said he could not support it. Since then, it has been slightly revised. City staff has recommended approval of the streetscape concept with the understanding that construction documents should not be developed until a way is found to mitigate the loss of the 33 parking spaces. In the meantime, a pilot project would be conducted.

We have been working on this for about two years and one of the next steps are to get this to council and have them approve the streetscape plan and then moving into the next plans which are schematic and construction documents. There are numerous steps to come forth as you all note here there is a conceptual plan to what could occur with limited data at this time. The next steps would commit more data to refining and making sure it works from an engineer prospective. She said there was a parking study done and a pilot program which would be a temporary option to put out some of the conceptual plan to test out the ideas such as the reduction of parking, the dedicated bicycle facilities and for a much lower cost and shorter time to track things out before or could happen concurrently. You might want to recommend one way or the other to Council. She said related to that is the parking improvement and if we were to do that we would have to find alternative ways to house parking. She said several of those components that they found to be appropriate the planning commission could recommend options.

Lisa Robertson, City Attorney said the Planning Commission does have a specific role in what type of plan and that is to express an opinion or as to whether you think that conceptual plans compare to the Comprehensive Plan. The Planning Commission has a purview to include a general statement of the goals in the Comprehensive Plan.

Commissioner Keesecker said he doesn't have an issue with losing that number of parking spaces as long as it is coupled with managed parking that was mentioned in the study. The trade-off seems fair to try to enhance this corridor.

Commissioner Green said she favored removing more parking spaces to make it safer for people riding bicycles.

Commissioner Lahendro said with a 60 foot right of way something has to give, parking has to be compromised.

Commissioner Santoski said on the street parking is a protective barrier, part of him sad to see all the parking go, but the on street parking is a perceived safety. He spoke about metered parking.

Ms. Rainey said the pilot program would basically take place between the curbs. We're talking about striping and testing out the removal of parking to make way for bike facilities.

Chairman Rosensweig said he thought the western side could use more strategic parking to help businesses. He said he didn't have any heartburn over losing the number of spots, but didn't think it was done very strategically. He also stated there are some places where losing parking is going to create economic dead zones.

Commissioner Keller said this is not the necessarily the plan she would have developed if she had been on the team. She said everyone needs to realize that this is a plan that has been put together to meet the needs of a lot of constituencies. One of the important ones is the University which was not mentioned and it was detrimental. The turning lanes maybe necessary to the hospital. She said she will be interested in hearing what UVA. representatives have to say. The process was not inclusive enough.

Chairman Rosensweig agreed the balance and scale is wrong.

Commissioner Santoski asked what will you lose if you put parking back into it and is it possible to cut a little off of everything and figure it out how to make it all fit in there together?

Ms. Creasy said this is similar to the discussions that have been held over the last two years. It is a literal battle to try to fit everything in and not everything is going to fit in. Choices were made to try to balance all of the different constituencies that were coming forward.

Commissioner Lahendro said we need more left hand turn lanes.

Carrie Rainey said there is a preliminary parking study being done. There medians are transversal, something that is transversal is for people to try to think that something is there. Commissioner Keesecker said he thought a random meeting to discuss the Master Plan with Council and the traffic study around the hospital because he felt the Roosevelt Brown failed on all accounts. That intersection had its issues all the way over to 11th Street.

Commissioner Keesecker said in the BAR meeting one of the comments came from Mrs. Knott, she thought the plan would do well if the relationship of this was incorporated into the neighborhoods, the network effect to get to what we went back and forth may be the only criticism he has were her thoughts.

Commissioner Santoski said he thought parking would be available for the public at the Flats.

Commissioner Keller made a recommendation that the traffic flow and other problematic issue, and the center median be planted. Something more creative within the median.

Commissioner Green is not convinced that that adds life to the street.

Chairman Rosensweig wanting to pend down applications, cafes places where people can work in and out unless we have some parallel on the street.

Commissioner Keesecker if you start that kind of existence at UVA avenue on the east side, at present a continece on north side of West Main, maybe need a little more detail on the western portion to further define itself.

Commissioner Santoski regarding the side streets to make sure there is parking on them, metered to allow people to park. Can we do something with the side streets, operating on the assumption that there would be parking in the university housing?

Ms. Rainey said we did not need a parking garage at this time.

Commissioner Keller said the parking study is comprehensive, smart parking for W Main and Downtown.

Commissioner Santoski said the bike lanes could be shared with a sidewalk.

Commissioner Green said no bike lanes can be shared with a sidewalk.

Commissioner Keesecker on the north side there is a number of properties have perpendicular parking right into their front yard. If there could be some expression to take advantage of the wasted street space.

Commissioners spent much of the meeting debating the details of the plan, including whether the sidewalks and bike lanes could be slightly narrower.

Commissioner Santoski said one of the frustrating things to him with any of our transportation stuff is that we always go back to the size of the fire vehicles we need to have and what we need to have for turning radiuses. He stated that there's got to be a different way to do to this.

Commissioner Green said we need to look at the safety issues.

Commissioner Keller added a few softer creative aspects, valet parking is a good one, and reference to improving bus and trolley service would be used more. She would much want to use the trolley.

Commissioner Keesecker endorses a Pilot Program with whatever methods needed

Commissioner Lahendro did not agree with the pilot program but regarding the trees and utilities don't plan into making a decision.

Commissioner Green asked how the construction will be done.

Carrie Rainey said until we get direction from Council, it won't be a quick and easy

There was consensus to support the pilot program, but some expressed concern that about what would actually be done as the pilot.

There was also consensus to have the commission recommend high-level conversations to get Dominion to help pay to put their lines underground.

Commissioner Keesecker said in the BAR there is different between utility lines and the Belmont chop, the grid can be distinguished. They don't care if they go through the trees or not. Nice to have their input about the lines in the street. A common practiced a long in your front-yard; a number places thy put in a pole and leave the old pole there.

Commissioner Keller she really can't believe that someone in the city can't go to Dominion Power and talk to the right person and it is not her. If the right person stayed persistent it could happen.

Commissioner Keesecker it is important to West Main, how the utility are dealt with on this street.

Commissioner Lahendro said parks and recreation needs educating when it comes to trees.

Brian Haluska, Principal Planner said the way it is generally done in the past, when it is a redevelopment we get one stretch, but it hasn't been a cohesive effort to do a block. He said the improvements will require a big project all at once. In prior plans, the streetscape would come piece by piece, but only recently has the City seen a large amount of construction on West Main Street.

Ms. Rainey said pedestrian lighting certainly will be a part of the project moving forward.

Public Comments

Elizabeth Waters, 1935 Thomson Road: stated the Tree Commission is in support. We care about soil volume. We were not on the advisory committee, whatever is endorse conceptually, Pilots testing is a lot of voices quite loud some night heard as much, preserve and use the businesses along there, encourage to recognize the balance. The Tree Commission does feel strongly that development as they go forward, has some peace-meal to make sure we get some rules in place and a lot of money in this bigger concept plan, a lot of bearings.

Kevin Fox 195 Lankford Place said from the Medical Center's perspective, the unimpeded flow of emergency vehicles on West Main Street is critical to our operations, and the facilities administrator for UVa Medical Center. He also took issue with the concept in the plan to have buses stop within the travel lane rather than the pull-off areas that exist in locations near UVa. He stated having buses drop people off in the drive lanes doesn't foster a conducive traffic flow on the street.

Mary Hughes, interim architect at UVa supports the goals of this plan. We welcome this plan and the opportunity in playing a more active role with this project. She said the community wants this on West Main. She asked that a thorough traffic analysis of the entire corridor be conducted before any more detailed design goes forward.

Members of the bike community pushed for protected lanes that separate bikes and pedestrians. The plan features this element in some locations but in some areas bikes and pedestrians are in the same general area at the same level.

Ruth Stornetta, 307 C Second Street NW, said the biggest concerned is safety issues, doors opening, vehicles in the bike lanes, runners and pedestrian in the bike lane. It is ignored by the buses, cars and pedestrians. She said the idea of sharing space with bikes and pedestrians really does not work for people who are commuting by bike on West Main. By moving back and forth between protected and non-protected bike lanes it's kind of doing a disservice to the people you're trying to get to commute on a bike.

Eberhard Jehle 1402 Hazel Street endorses what Ruth has said, our thing is Charlottesville is a world class city. Health and safety of pedestrians needs to make the backbone functioning, West Main friendly, to bike, to walk, hope the planning commission supports the consultant recommend Pilot study to protect the facilitate community support. 5% of the parking at the train station 281, the median west toward the UVA and the dollars involved in the underground utilities.

Peter Ohlms- thinks what Ruth said is valuable, even with the RH plan. The protective bike lanes should be on both sides. He hopes it can go forth quickly.

2. West Main Street Zoning Code Draft Considerations

Ms. Creasy stated Council sent this with a draft and the planning commission can talk about it as long as you need too.

Commissioner Keller said she would like to see them move on this as quickly as we could.

Ms. Creasy said this could be your discussion and if you find the draft before you is in good shape with a few corrections then we can schedule it sooner rather than later.

Elizabeth Waters – said to include landscape, zoning might go forward before the streetscape. Required to bring buildings to the street, looking at the zoning and she hopes the proposal before you does not have landscaping involved, and hopes someone will say something about planting within the 15 feet.

Ms. Rainey said they are required to do trees.

Morgan Butler excited to see this, and wants this to be a priority for Council with the existing zoning. A lot of work has gone into this work before you tonight. Council has called this list into a set of changes that would not be implementing as form based code. He thinks staff has done a good job. There are eleven different issues to discuss. Exterior boundaries, differentiation, make sure the BAR governs that.

Ms. Robertson agreed that there is an issue with the language requiring the articulation of the façade of a building, specifically how vague this language is. She said we need to be thinking about some more concrete language how to articulate how the zoning language should be.

3. Development Review Policy:

Lena Seville, 808 AltaVista Avenue commented on meetings with public comments could possibly be structured similar to what Albemarle County does.

Ms. Robertson noted that the waiver policy the ordinance is based on Albemarle County's ordinance. The purpose was to develop a policy that council is asking everyone to consider to guide staff in making decisions in terms of what circumstances could the meeting requirement be waived.

Adjourn 7:00

CITY OF CHARLOTTESVILLE
DEPARTMENT OF NEIGHBORHOOD DEVELOPMENT SERVICES
STAFF REPORT

**APPLICATION FOR APPROVAL OF
PRELIMINARY SITE PLAN**

PLANNING COMMISSION REGULAR MEETING
DATE OF PLANNING COMMISSION MEETING: August 11, 2015

Author of Staff Report: Matt Alfele

Date of Staff Report: July 27, 2015

Project Name: 1725 JPA Apartments

Applicant: Scott Collins, Collins Engineering

Applicant's Representative: Scott Collins, Collins Engineering

Applicable City Code Provisions: 34-800 – 34-827 (Site Plans), 34-867 (Landscape Plan)

Zoning District: R-3 (Multifamily Residential) and Entrance Corridor

Date of Preliminary Site Plan Submission: February 24, 2015

Date of Site Plan Review Conference: March 18, 2015

Reason for Planning Commission Review: All Site Plans associated with a property which has a Special Use Permit are subject to review by the Planning Commission

Vicinity Map



Legal Standard of Review

Approval of a site plan is a ministerial function, as to which the Planning Commission has little or no discretion. When an applicant has submitted a site plan that complies with the requirements of the City's Site Plan Ordinance, then approval of the plan must be granted. In the event the Planning Commission determines there are grounds upon which to deny approval of a site plan, the motion must clearly identify the deficiencies in the plan, that are the basis for the denial, by reference to specific City Code sections and requirements. Further, upon disapproval of a site plan, the Planning Commission must identify the modifications or corrections that would permit approval of the plan.

Executive Summary

Scott Collins of Collins Engineering, acting as agent for Neighborhood Investments, LLC is requesting approval of a preliminary site plan to construct a (19) unit apartment building at the intersection of Jefferson Park Avenue and Montebello Circle (TMP 16, 16) City Council approved a Special Use Permit (SP-1500001) for additional density and modifications to side setback on June 1, 2015

On July 14, 2015 the Entrance Corridor Review Board approved (5-0) a Certificate of Appropriateness with a condition that the applicant would work with staff to use appropriate stone retaining walls if any retaining is required to resolve elevation changes as the final site plan comes together.

Staff recommends approval, conditioned on the satisfaction of remaining comments during the final site plan review process.

Site Plan Compliance

The preliminary site plan is currently under review, and the applicant will be required to comply with staff comments. There have been several rounds of review by City reviewers. Site plans are reviewed for compliance with City codes and standards. An overview of site plan requirements and the location of those items on the site plan are outlined below.

Site Plan Requirements

A. Compliance with applicable zoning district regulation

Residential R-3 (*per Zoning Ordinance §34-350 - - §34-421*)

The property was rezoned to R-3 in 1958. The project complies with all requirements of the R-3 Multiple Dwelling District.

B. Compliance with the City’s Erosion and Sediment Control ordinance, City Code, Chapter 10:

The applicant’s erosion and sediment control plan will be submitted and reviewed during final site plan submission. The applicant will be required to comply with staff comments.

**C. Compliance with General Standard for site plans (Sections 34-800 through 34-827)
Section 34-827 Preliminary site plan contents**

1. General site plan information, including but not limited to project, property, zoning, site, and traffic information: **Found on Sheet 1.**
2. Existing condition and adjacent property information: **Found on Sheet 2.**
3. There is no phasing for the project.
4. Topography and grading: **Found on Sheet 1 and Sheet 3.**
5. Existing landscape and trees: **Found on Sheet 2.**
6. The name and location of all water features: **N/A.**
7. One hundred-year flood plain limits: **N/A.**
8. Existing and proposed streets and associated traffic information: **Trip generation numbers are shown on Sheet 1 of the site plan. No new roads are proposed.**
9. Location and size of existing water and sewer infrastructure: **Found on Sheet 2.**
10. Proposed conceptual layout for water and sanitary sewer facilities and storm drain facilities: **Found on Sheet 1 and Sheet 3.**
11. Location of other existing and proposed utilities and utility easements: **Found on Sheet 1 and Sheet 3.**
12. Location of existing and proposed ingress to and egress from the property, showing the distance to the centerline of the nearest existing street intersection: **Found on Sheet 1, Sheet 3, and Sheet 4.**
13. Location and dimensions of all existing and proposed improvements: **Found on Sheet 1 and Sheet 3.**
14. All areas intended to be dedicated or reserved for public use: **N/A.**
15. Landscape plan: **Found on Sheet 3.**
16. Where deemed appropriate by the director due to intensity of development:
 - a. Estimated traffic generation figures for the site based upon current VDOT rates: **Found on Sheet 1.**
 - b. Estimated vehicles per day: **Found on Sheet 1.**

D. Additional information to be shown on the preliminary site plan as deemed necessary by the director or Commission in order to provide sufficient information for the director or Commission to adequately review the preliminary site plan.

The applicant needs to show the Special Use Permit conditions on the coversheet of the site plan.

E. Compliance with Additional Standards for Specific Uses (*Site Plan Ordinance §§34-930 - - 34-938*)

The plan complies with the additional standards required for dumpsters and parking garages.

Public Comments Received

Several members of the public have been involved throughout the review, particular with regards to the Special Use Permit, height of the building, and overall aesthetics in relations to the Jefferson Park Avenue Neighborhood.

Recommendation

Staff recommends approval of the preliminary site plan with the following condition:

- All remaining staff comments from the preliminary site plan review must be satisfied during the final site plan review as outlined in the comment letter dated July 2, 2015

Attachments

- Preliminary Site Plan Dated June 12, 2015
- Staff Site Plan Comment Letter Dated July 2, 2015
- Special Use Permit Resolution Dated June 1, 2015
- Three-dimensional model per Sec. 34-827(a)
- Entrance Corridor Application Materials (July 14, 2015)
<http://www.charlottesville.org/index.aspx?page=3657>



1725 JPA APARTMENTS

PRELIMINARY SITE PLAN AND SPECIAL USE PERMIT
CITY OF CHARLOTTESVILLE, VIRGINIA

PROJECT DATA:

- THE OWNER/CLIENT OF THIS PROPERTY IS:
NEIGHBORHOOD INVESTMENTS, LLC
810 CATALPA COURT
CHARLOTTEVILLE, VA 22903
- THESE PLANS HAVE BEEN PREPARED BY:
COLLINS ENGINEERING, LLC
200 GARRETT STREET, SUITE K
CHARLOTTEVILLE, VA 22902
TELEPHONE: (434) 293-3719
- SOURCE OF SURVEY, TOPOGRAPHY & BOUNDARY INFORMATION: COMMONWEALTH LAND SURVEYING IN JANUARY 2015. FIELD VERIFIED BY COLLINS ENGINEERING FEBRUARY 2015.
- ZONING: R-3 WITH SPECIAL USE PERMIT (SP15-00001) FOR MODIFICATIONS TO THE SIDE YARD SETBACKS AND DENSITY
- SPECIAL USE PERMIT: SP15-00001 WAS APPROVED JUNE 1, 2015 AND PERMITTED AN INCREASE IN DENSITY FROM 1-21 DU/ACRE TO 44-64 DU/ACRE AND A REDUCTION OF SIDE YARD SETBACKS FROM 1' PER EVERY 4' OF HEIGHT (MINIMUM 10') TO 5'. THE CONDITIONS OF APPROVAL ARE PROVIDED ON THIS SHEET.
- THIS PROJECT FALLS WITHIN AN ENTRANCE CORRIDOR REVIEW
- TAX MAP AND PARCEL NUMBER: TMP 160016000
- USGS DATUM: NAD 83 (1994)
- LOCATION/ADDRESS OF PROJECT: 1725 JEFFERSON PARK AVENUE, CHARLOTTEVILLE VA 22903
- BUILDING HEIGHT: MAXIMUM BUILDING HEIGHT OF 45 FEET (PER R-3 ZONING)
- PROPOSED USE:
19 UNIT APARTMENT BUILDING
4 BEDROOM APARTMENTS: 13 UNITS
2 BEDROOM APARTMENTS: 6 UNITS
- GROSS DENSITY: ALLOWED BY SPECIAL USE PERMIT: (44-64 DUA) = 24 UNITS MAX
PROPOSED DENSITY: 19 UNITS (49 DUA)
- TOTAL ACREAGE OF SITE: 0.385 ACRES
- TOTAL PROJECTED LAND DISTURBANCE: 0.385 ACRES
- SITE PHASING: ONE PHASE
- CRITICAL SLOPES: NONE
- AMENITIES:
LAUNDRY: 19 WASHERS AND DRYERS (ONE PER UNIT)
STORAGE: 192 SF PROVIDED SPLIT BETWEEN THE GARAGES AND CORRIDORS (192 SF REQ'D = 3 SF PER BEDROOM X 64 BEDROOMS)
REC AREA: 4,921 SF OPEN/LAWN/HARDSCAPE PROVIDED (25% TO BE INDOOR OR WEATHER PROTECTED TO BE DETERMINED WITH FINAL SITE PLAN) REQUIRED: 3800 SF ADULT SPACE + 900 SF MIXED USE SPACE=[13 FOUR BEDROOM X [200 SF ADULT + 60 SF MIXED SPACE]] + [6 TWO BEDROOM X [200 SF ADULT + 20 SF MIXED SPACE]] 25% OF THE RECREATION AREA MUST BE INDOOR OR WEATHER PROTECTED
- BICYCLE PARKING REQUIREMENTS: (18 REQUIRED & 20 PROVIDED = 8,742 SF BEDROOM AREA X 1 SPACE PER 500 SF BEDROOM AREA)
- PARKING REQUIREMENTS:
REQUIRED PARKING:
13 FOUR BD RM UNITS = 26 REQUIRED SPACES
6 TWO BD RM UNITS = 6 REQUIRED SPACES
TOTAL REQUIRED SPACES = 32 REQUIRED SPACES
PROPOSED PARKING:
32 GARAGE SPACES
0 SURFACE PARKING SPACES
0 OFFSITE PARKING SPOTS
TOTAL PARKING PROVIDED: 32 SPACES
- PUBLIC UTILITIES: THE SITE WILL BE SERVED BY EXISTING PUBLIC WATER AND SEWER. WATER AND WASTE WATER MAIN PROFILES WILL BE PROVIDED WITH THE FINAL SITE PLAN.
- STORMWATER MANAGEMENT AND STORM DRAINAGE: THE STORMWATER RUNOFF RATES, VOLUMES, AND VELOCITIES RESULTING FROM THIS DEVELOPMENT WILL BE IMPROVED PRIOR TO ENTERING THE CITY'S STORM SEWER SYSTEM. THE DEVELOPMENT CURRENTLY PROPOSES THE INSTALLATION OF PERMEABLE PAVEMENT FOR THE EXTERIOR PARKING LOT WITH A DOWNGRADED BIORETENTION BASIN PROPOSED TO TREAT THE ROOFTOP DRAINAGE. THIS SWM PLAN, OR AN EQUIVALENT SWM PLAN MEETING MINIMUM CITY REQUIREMENTS, SHALL BE PROPOSED AND REVIEWED WITH THE FINAL SITE PLAN.
- STREAM BUFFER: THE DEVELOPMENT OF THIS PROPERTY DOES NOT IMPACT A STREAM BUFFER.
- SIGNAGE: SITE SIGNAGE SHALL BE SUBMITTED UNDER A SEPARATE APPLICATION.
- STREET CLOSURE: A TEMPORARY STREET CLOSURE PERMIT IS REQUIRED FOR CLOSURE OF SIDEWALKS, PARKING SPACES AND ROADWAYS AND IS SUBJECT TO APPROVAL BY THE CITY TRAFFIC ENGINEER. PARTIAL STREET CLOSURES WILL BE NEEDED FOR THE CREATION OF THE SITE ENTRANCE AT 5TH STREET AND IMPROVEMENTS TO CLEVELAND AVENUE.
- INGRESS AND EGRESS: ACCESS TO THIS PROPERTY SHALL BE PROVIDED VIA STREET ACCESS AT MONTEBELLO CIRCLE.
- BUILDING/LOT SETBACKS:
FRONT - 25'
SIDE - 5' (PER APPROVED SPECIAL USE PERMIT)
REAR - 25'
- LIGHTING SHALL BE DETERMINED WITH THE FINAL SITE PLAN. LIGHTING SHALL MEET THE CITY DARK SKY ZONING ORDINANCE, BE FULL CUT-OFF LUMINAIRES, AND LIGHTING SHALL BE PROPOSED AT BUILDING ENTRANCES ONLY.
- SITE TRIP GENERATION AND LAND USE ITE CODE. BASED ON THE ITE TRIP GENERATION MANUAL, 8TH EDITION.
APARTMENT (19 UNITS)
AVERAGE DAILY TRIPS: 126 VPD (63 VPD ENTER/63 VPD EXIT)
AM PEAK RATE: 10 VPH (2 VPH ENTER/8 VPH EXIT)
PM PEAK RATE: 12 VPH (8 VPH ENTER/4 VPH EXIT)
- WATER FLOW CALCULATIONS
MAX HOUR Q=11.4*Q_{0.544}; N=19; Q=56.56 GPM. FIRE FLOW SHALL BE 1500 GPM MINIMUM.
- SEWER FLOW CALCULATIONS
64 APARTMENT BEDS X 100 CPD = 6,400 CPD
FIRE FLOW TESTING RESULTS: GPM: 1350 STATIC: 104 PSI RESIDUAL: 100 PSI
EFFECTIVE AREA: 8,800 SF+(5*4,200 SF)+(5*4*8,800)=28,500 SF(LARGEST FLOOR + HALF REMAINING FLOOR AREAS)
COEFFICIENT FACTOR: 0.8 (NONCOMBUSTIBLE)
CONSTRUCTION FACTOR: 2500 GPM
OCCUPANCY FACTOR: 0.85 (CLASS 2 APARTMENT)
EXPOSURE FACTOR: WEST, EAST, SOUTH SIDE: EXPOSURE GREATER THAN 40' Xi=0; NORTH SIDE: DISTANCE TO BUILDING =10' HOWEVER EXPOSURE BUILDINGS RATE AS HAZARDOUS Xi=0
COMMUNICATIONS FACTOR: Pi=0 (NO PASSAGEWAYS ON SIDES WITH EXPOSURES LESS THAN 50')
NFF= 2250 GPM
- EXISTING VEGETATION: SMALL TREES AND SHRUBS COVER A PORTION OF THE SITE.
- STREAM BUFFER MITIGATION PLAN: NOT APPLICABLE
- CONSERVATION PLAN: NOT APPLICABLE
- PAVED PARKING AND CIRCULATION AREA: 9,490 SF
- IMPERVIOUS AREAS: PREDEVELOPMENT: BUILDING: 2,565 SF PAVEMENT: 5,505 SF SIDEWALKS: 375 SF
POSTDEVELOPMENT: BUILDING: 8,825 SF PAVEMENT: 1,120 SF SIDEWALKS: 3,385 SF

- NOTES:**
- ALL SIGNAGE AND PAVEMENT MARKINGS SHALL BE CONSISTENT WITH THE MUTCD.
 - IF 505-THE BUILDING STREET NUMBER TO BE PLAINLY VISIBLE FROM THE STREET FOR EMERGENCY RESPONDERS.
 - IF 506-1-AN APPROVED KEY BOX SHALL BE MOUNTED TO THE SIDE OF THE FRONT OR MAIN ENTRANCE. THE CHARLOTTEVILLE FIRE DEPARTMENT CARRIES THE KNOX BOX MASTER KEY. A KNOX BOX KEY BOX CAN BE ORDERED BY GOING ONLINE TO WWW.KNOXBOX.COM. THE KNOX BOX ALLOWS ENTRY TO THE BUILDING WITHOUT DAMAGING THE LOCK AND DOOR SYSTEM.
 - STRUCTURES WITH FIRE PROTECTION SYSTEMS SHALL INDICATE THE LOCATION OF ANY FIRE LINE TO THE BUILDING(S) AS WELL AS THE LOCATION OF FIRE DEPARTMENT CONNECTIONS.
 - FIRE HYDRANTS, FIRE PUMP TEST HEADER, FIRE DEPARTMENT CONNECTIONS OR FIRE SUPPRESSION SYSTEM CONTROL VALVES SHALL REMAIN CLEAR AND UNOBSTRUCTED BY LANDSCAPING, PARKING OR OTHER OBJECTS. THE FIRE MARSHAL'S OFFICE NO LONGER ALLOWS ANY TYPE OF LANDSCAPING TO BE PLACED IN FRONT OF AND WITHIN 5 FEET OF FIRE HYDRANTS, FIRE PUMP TEST HEADERS, FIRE DEPARTMENT CONNECTIONS OR FIRE SUPPRESSION SYSTEM CONTROL VALVES. AN APPROVED WATER SUPPLY FOR FIRE PROTECTION SHALL BE MADE AVAILABLE AS SOON AS COMBUSTIBLE MATERIAL ARRIVES ON THE SITE.
 - ALL PAVEMENT SHALL BE CAPABLE OF SUPPORTING FIRE APPARATUS WEIGHING 75,000 LBS.
 - IF 1404.1-SMOKING IS TO BE ALLOWED IN ONLY DESIGNATED SPACES WITH PROPER RECEPTACLES.
 - IF 1404.2-WASTE DISPOSAL OF COMBUSTIBLE DEBRIS SHALL BE REMOVED FROM THE BUILDING AT THE END OF EACH WORKDAY.
 - IF 1410.1-ACCESS TO THE BUILDING DURING DEMOLITION AND CONSTRUCTION SHALL BE MAINTAINED.
 - IF 1404.6-CUTTING AND WELDING. OPERATIONS INVOLVING THE USE OF CUTTING AND WELDING SHALL BE DONE IN ACCORDANCE WITH CHAPTER 26, OF THE INTERNATIONAL FIRE CODE ADDRESSING WELDING AND HOTWORK OPERATIONS.
 - IF 1414.1-FIRE EXTINGUISHERS SHALL BE PROVIDED WITH NOT LESS THAN ONE APPROVED PORTABLE FIRE EXTINGUISHER AT EACH STAIRWAY ON ALL FLOOR LEVELS WHERE COMBUSTIBLE MATERIALS HAVE ACCUMULATED.
 - REQUIRED VEHICLE ACCESS FOR FIRE FIGHTING SHALL BE PROVIDED TO ALL CONSTRUCTION OR DEMOLITION SITES. VEHICLE ACCESS SHALL BE PROVIDED TO WITHIN 100 FEET OF TEMPORARY OR PERMANENT FIRE DEPARTMENT CONNECTIONS. VEHICLE ACCESS SHALL BE PROVIDED BY EITHER TEMPORARY OR PERMANENT ROADS, CAPABLE OF SUPPORTING VEHICLE LOADING UNDER ALL WEATHER CONDITIONS. VEHICLE ACCESS SHALL BE MAINTAINED UNTIL PERMANENT FIRE APPARATUS ACCESS ROADS ARE AVAILABLE.
 - OVERHEAD WIRING OR OTHER OBSTRUCTIONS SHALL BE HIGHER THAN 13 FEET 6 INCHES.
 - ALL SIGNS SHALL BE IN ACCORDANCE WITH ARTICLE IX, SECTION 34-1020, CITY CODE.
 - VSPFC 905.3.1 - IF THE FLOOR LEVEL OF THE HIGHEST STORY IS MORE THAN 30 FEET ABOVE THE LOWEST LEVEL OF FIRE DEPARTMENT VEHICLE ACCESS, THEN A CLASS I STANDPIPE SYSTEM MUST BE INSTALLED IN ADDITION TO THE SPRINKLER SYSTEM.
 - VSPFC 3311.1 - WHERE A BUILDING HAS BEEN CONSTRUCTED TO A HEIGHT GREATER THAN 50 FEET OR FOUR (4) STORIES, AT LEAST ONE TEMPORARY LIGHTED STAIRWAY SHALL BE PROVIDED UNLESS ONE OR MORE OF THE PERMANENT STAIRWAYS ARE ERRECTED AS THE CONSTRUCTION PROGRESSES.
 - VSPFC 3313.1 - BUILDINGS FOUR OR MORE STORIES IN HEIGHT SHALL BE PROVIDED WITH NOT LESS THAN ONE STANDPIPE FOR US DURING CONSTRUCTION. SUCH STANDPIPES SHALL BE INSTALLED WHEN THE PROGRESS OF CONSTRUCTION IS NOT MORE THAN 40 FEET IN HEIGHT ABOVE THE LOWEST LEVEL OF FIRE DEPARTMENT ACCESS. SUCH STANDPIPE SHALL BE PROVIDED WITH FIRE DEPARTMENT HOSE CONNECTIONS AT ACCESSIBLE LOCATIONS ADJACENT TO USABLE STAIRS. SUCH STANDPIPES SHALL BE EXTENDED AS CONSTRUCTION PROGRESSES TO WITHIN ONE FLOOR OF THE HIGHEST POINT OF CONSTRUCTION HAVING SEALED DECKING OR FLOORING.
 - QUADRANTS REQUIRED AT THE TOP OF ALL RETAINING WALLS WITH A GRADE DIFFERENCE EXCEEDING 30°.
 - HANDRAILS REQUIRED AT BOTH SIDES OF STAIRS.
 - 5' SIDE SETBACKS HAS A RESTRICTIVE BUILDING CODE REQUIREMENT FOR % OPENINGS AND EXTERIOR WALL FIRE RATINGS. THESE CALCULATIONS WILL BE SHOWN ON THE BUILDING PLANS.
 - A MINIMUM OF 98" HEIGHT CLEARANCE IS REQUIRED AT PARKING GARAGE DOORS AND CLEARANCE AT HANDICAP PARKING SPACES.

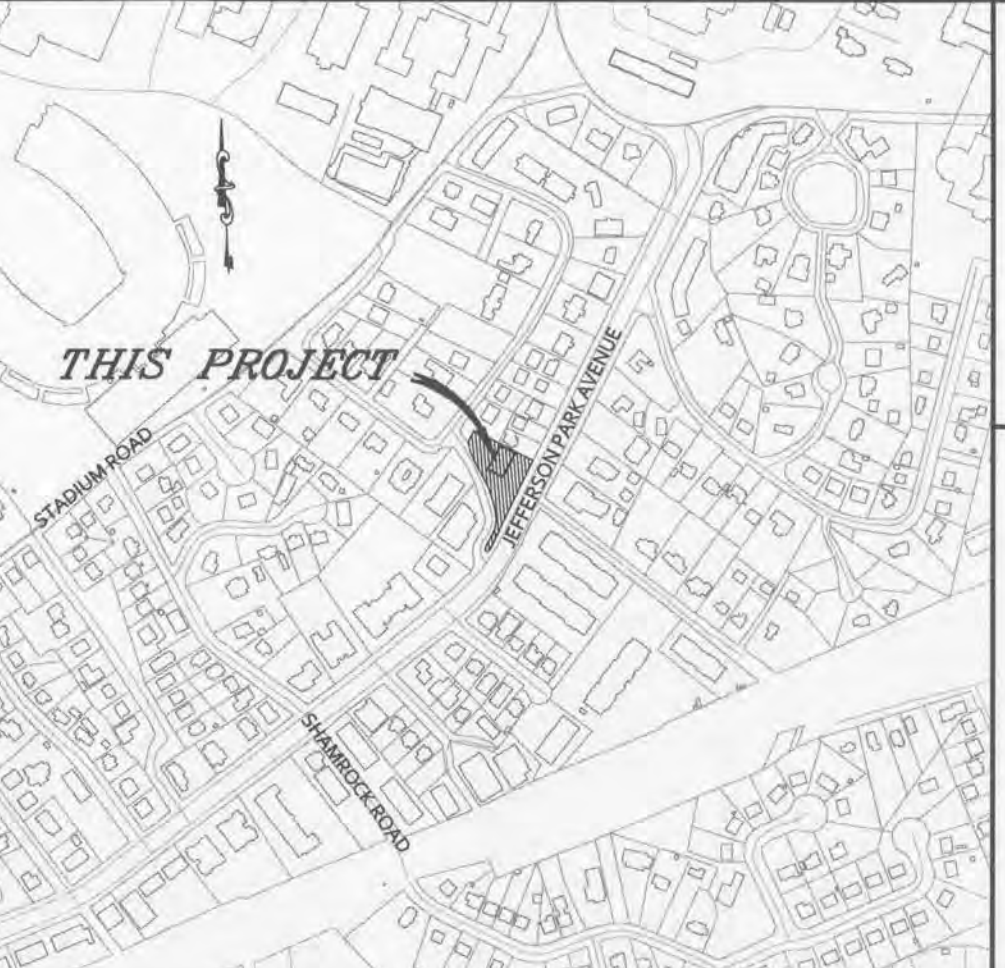
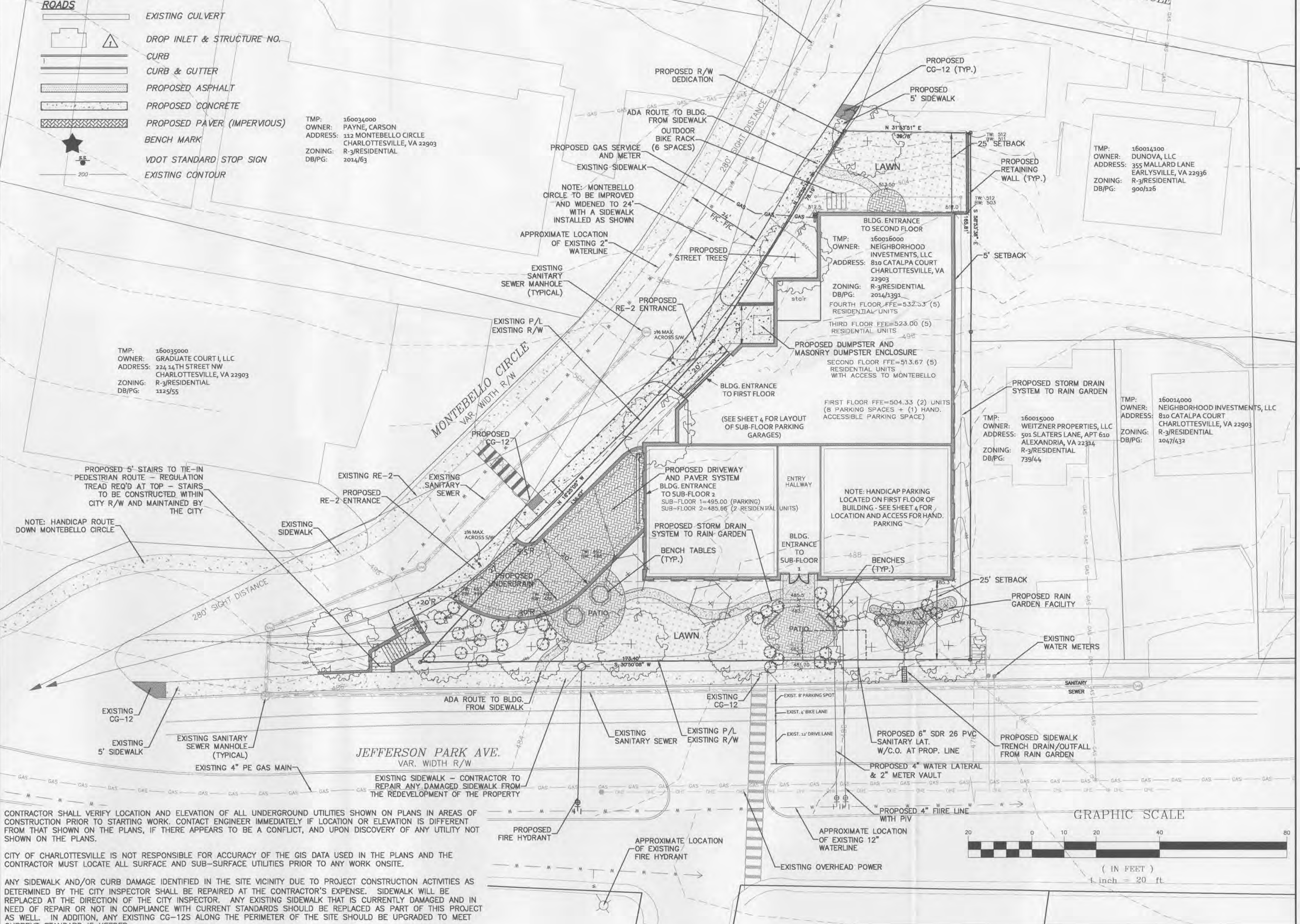
Sheet List Table	
Sheet Number	Sheet Title
1	COVER
2	EXISTING CONDITIONS & DEMOLITION PLAN
3	SITE & LANDSCAPING PLAN
4	NOTES & DETAILS
4	TOTAL SHEETS

CONDITIONS OF APPROVAL OF SPECIAL USE PERMIT SP15-00001

- Conform to *See 34-881-Bicycle Storage Facilities* on the most current Bicycle Storage Facilities code at time of development.
- The finished floor elevation (FFE) and building entrance shall be no more than (6.5) feet above the average elevation of Jefferson Park Avenue than runs in front of the property.
- Street trees shall be required as depicted with the application materials dated April 21, 2015, submitted to the City for and in connection with SP-1500001 ("Application") and be 4" caliper at planting.
- The design height, density, and other characteristics of the development shall remain essentially the same. In all material aspects, as described within the application materials dated April 21, 2015, submitted to the City for and in connection with SP-1500001 ("Application"). Except as the design details of the development may subsequently be modified to comply with requirements of a certificate of appropriateness issued by the City's Entrance Corridor Review, staff comments, or by any other provision(s) of these SUP Conditions, any change of the development that is inconsistent with the application shall require a modification of this SUP.
- All outdoor lighting and light fixtures shall be full cut-off luminaires.
- If the developer elects to make a contribution to the City's Affordable Housing Fund to satisfy City Code 34-12(d)(2), no building permit shall be issued for the development until the amount of the contribution is calculated by the Director of Neighborhood Development Services, or designee, and until such contribution has been paid in full to the City. If the developer elects to satisfy City Code 34-12(d)(1) a detailed plan must be submitted and approved by the Director of Neighborhood Development Services, or designee before a building permit is issued.

LEGEND

- ROADS**
- EXISTING CULVERT
 - DROP INLET & STRUCTURE NO.
 - CURB
 - CURB & GUTTER
 - PROPOSED ASPHALT
 - PROPOSED CONCRETE
 - PROPOSED PAVR (IMPERVIOUS)
 - BENCH MARK
 - VDOT STANDARD STOP SIGN
 - EXISTING CONTOUR



VICINITY MAP

SCALE: 1" = 500'

REVISIONS

REVISION DESCRIPTION	DATE
INITIAL SUBMITTAL	2/24/15
REVISED PER CLIENT CHANGES AND CITY COMMENTS	4/21/15
REVISED PER CITY COMMENTS	6/12/15

COLLINS ENGINEERING

200 GARRETT STREET, SUITE K - CHARLOTTESVILLE, VA 22902 - 434.293.3719

1725 JPA APARTMENTS PRELIMINARY SITE PLAN

COVER



JOB NO. 102045
SCALE 1" = 20'
SHEET NO. 1

SIGNATURE PANEL

DIRECTOR, NEIGHBORHOOD DEVELOPMENT

TMP: 160128000
OWNER: 1800 JPA ASSOCIATES
ADDRESS: 200 RESERVE BLVD, SUITE 200
CHARLOTTEVILLE, VA 22901
ZONING: R/JHD/RESIDENTIAL
DB/PG: 1659/724

ALL SIGNING AND PAVEMENT MARKINGS SHALL BE CONSISTENT WITH THE MUTCD.
A TEMPORARY STREET CLOSURE PERMIT IS REQUIRED FOR CLOSURE OF SIDEWALKS, PARKING SPACES AND ROADWAYS AND IS SUBJECT TO APPROVAL BY THE CITY TRAFFIC ENGINEER.
SITE AND BUILDING CONSTRUCTION SHALL MEET 2006 IBC SECTION 3409 FOR ACCESSIBILITY AND VA USBC 103.3 FOR CHANGE OF OCCUPANCY.

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REVISIONS

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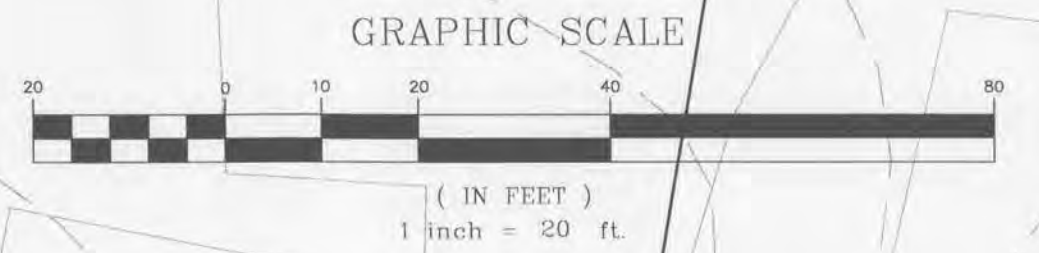
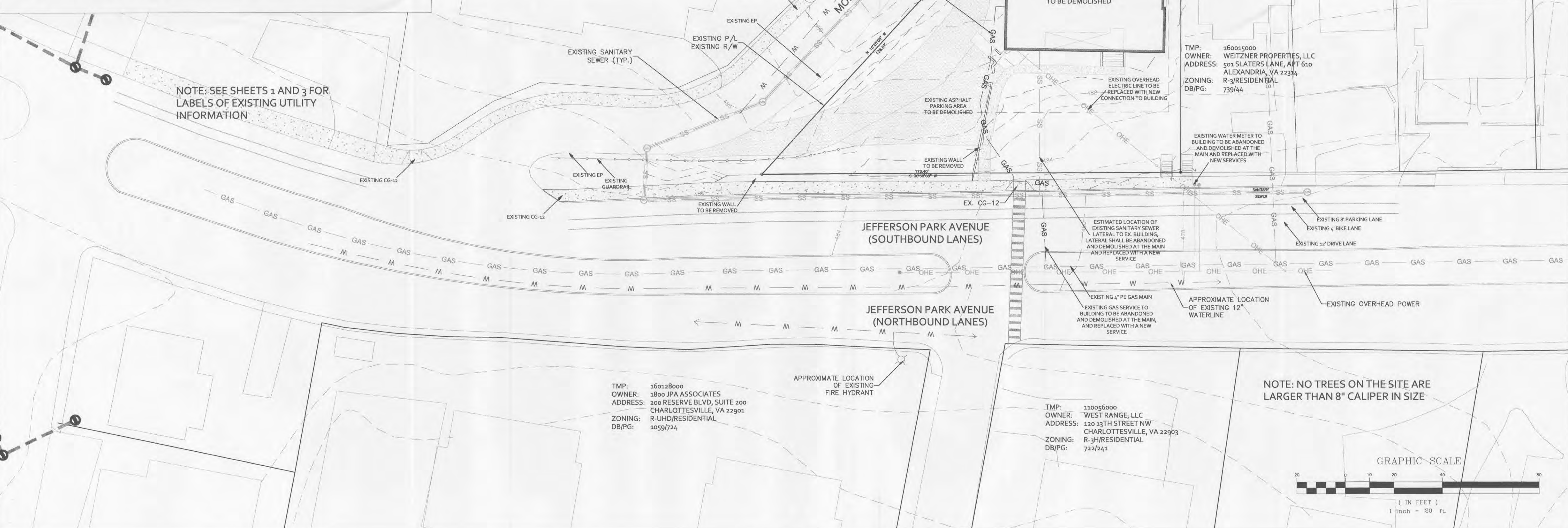
1725 JPA APARTMENTS PRELIMINARY SITE PLAN
EXISTING CONDITIONS & DEMOLITION PLAN

PROJECT SHEET
 JOB NO. 102045
 SCALE 1" = 20'
 SHEET NO. 2

- DEMOLITION NOTES:**
- PRIOR TO DEMOLITION AND CONSTRUCTION, A FIRE PREVENTION PLAN MEETING MUST OCCUR AND A FIRE PREVENTION PLAN MUST BE SUBMITTED TO AND APPROVED BY THE FIRE MARSHAL.
 - THE CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR ALL UNDERGROUND UTILITIES NOT SHOWN ON THIS PLAN SHEET AND SHALL DEMOLISH ALL DISCOVERED UTILITIES AS REQUIRED.
 - THE CONTRACTOR SHALL VIDEO AND INSPECT ALL SANITARY SEWER PIPES AND MANHOLES SLATED TO REMAIN TO DETERMINE ADEQUATE STRUCTURAL INTEGRITY. IF EXISTING SANITARY SEWER IS DAMAGED, THE CONTRACTOR SHALL CONTACT THE ENGINEER.
 - THE CONTRACTOR SHALL EXAMINE THE STRUCTURAL INTEGRITY OF EXISTING STORM SEWER STRUCTURES TO REMAIN AND REPLACE TOPS AS NECESSARY. THIS CONDITION SHALL BE REFLECTED IN THE CONTRACTOR BID.
 - ALL EXISTING WATER, SANITARY, AND STORM SEWER SLATED FOR DEMOLITION SHALL BE REMOVED FROM THE BUILDING TO THE PROPERTY LINE, UNLESS MARKED AS TO REMAIN.
 - UTILITIES THAT ARE DISCONNECTED SHALL BE PROPERLY ABANDONED AT THE MAIN LINE. FOR WATER SERVICE LINES, THE CORP STOP MUST BE TURNED OFF AT THE MAIN LINE AND THE SERVICE DISCONNECTED FROM THE MAIN. FOR SEWER LATERALS, THE LATERAL TAP MUST BE SEALED AT THE MAIN LINE SO THAT IT IS WATER TIGHT AND THE LATERAL REMOVED FROM THE MAIN LINE. FOR SANITARY MANHOLES TO BE ABANDONED THE TOP 2' OF THE MANHOLE STRUCTURE SHALL BE REMOVED, ALL LINES DISCONNECTED, AND THE MANHOLE SHOULD BE FILLED WITH STONE AND COVERED. ALL TAPS MUST BE LOCATED AND DISCONNECTED PER PROCEDURE ABOVE.
 - EXISTING ROOF DRAINS SLATED TO BE DEMOLISHED SHALL BE DISCONNECTED AND REMOVED; ROOFDRAINS TO BE REROUTED AS SHOWN ON THE ARCHITECTURAL PLANS.
 - EXISTING DOMINION OVERHEAD/UNDERGROUND ELECTRIC LINES AND OVERHEAD UTILITIES TO THE EXISTING BUILDING SHALL BE DISCONNECTED AND REROUTED AS PROPOSED ON THE UTILITY PLAN SHEET.
 - ANY EXISTING UNDERGROUND STORAGE TANKS SHALL BE DRAINED BY THE OWNER, AND THE CONTRACTOR SHALL FILL AND TANKS SHALL REMAIN.
 - VSPF 1404.1 - SMOKING TO BE ALLOWED IN ONLY DESIGNATED SPACES WITH PROPER RECEPTACLES.
 - VSPF 1404.2 - WASTE DISPOSAL OF COMBUSTIBLE DEBRIS SHALL BE REMOVED FROM THE BUILDING AT THE END OF EACH WORKDAY.
 - VSPF 1404.3 - ACCESS TO THE BUILDING DURING DEMOLITION AND CONSTRUCTION SHALL BE MAINTAINED.
 - VSPF 1404.6 - CUTTING AND WELDING. OPERATIONS INVOLVING THE USE OF CUTTING AND WELDING SHALL BE DONE IN ACCORDANCE WITH CHAPTER 26, OF THE INTERNATIONAL FIRE CODE, ADDRESSING WELDING AND HOTWORK OPERATIONS.
 - VSPF 1414.1-FIRE EXTINGUISHERS SHALL BE PROVIDED WITH NOT LESS THAN ONE APPROVED PORTABLE FIRE EXTINGUISHER AT EACH STAIRWAY ON ALL FLOOR LEVELS WHERE COMBUSTIBLE MATERIALS HAVE ACCUMULATED.
 - REQUIRED VEHICLE ACCESS FOR FIRE FIGHTING SHALL BE PROVIDED TO ALL CONSTRUCTION OR DEMOLITION SITES. VEHICLE ACCESS SHALL BE PROVIDED TO WITHIN 100 FEET OF TEMPORARY OR PERMANENT FIRE DEPARTMENT CONNECTIONS. VEHICLE ACCESS SHALL BE PROVIDED BY EITHER TEMPORARY OR PERMANENT ROADS, CAPABLE OF SUPPORTING VEHICLE LOADING UNDER ALL WEATHER CONDITIONS. VEHICLE ACCESS SHALL BE MAINTAINED UNTIL PERMANENT FIRE APPARATUS ACCESS ROADS ARE AVAILABLE.
 - VSPF 1408.1 PROGRAM SUPERINTENDENT. THE OWNER SHALL DESIGNATE A PERSON TO BE THE FIRE PREVENTION PROGRAM SUPERINTENDENT WHO SHALL BE RESPONSIBLE FOR THE FIRE PREVENTION PROGRAM AND ENSURE THAT IT IS CARRIED OUT THROUGH COMPLETION OF THE PROJECT. THE FIRE PREVENTION PROGRAM SUPERINTENDENT SHALL HAVE THE AUTHORITY TO ENFORCE THE PROVISIONS OF THIS CHAPTER AND OTHER PROVISIONS AS NECESSARY TO SECURE THE INTENT OF THIS CHAPTER. WHERE GUARD SERVICE IS PROVIDED, THE SUPERINTENDENT SHALL BE RESPONSIBLE FOR THE GUARD SERVICE.
 - VSPF 1408.2 PREFIRE PLANS. THE FIRE PREVENTION PROGRAM SUPERINTENDENT SHALL DEVELOP AND MAINTAIN AN APPROVED PREFIRE PLAN IN COOPERATION WITH THE FIRE CHIEF. THE FIRE CHIEF AND THE FIRE CODE OFFICIAL SHALL BE NOTIFIED OF CHANGES AFFECTING THE UTILIZATION OF INFORMATION CONTAINED IN SUCH PREFIRE PLANS.
 - A SITE SPECIFIC FIRE PREVENTION PLAN SHALL BE SUBMITTED TO THE FIRE MARSHAL'S OFFICE PRIOR TO COMMENCEMENT OF ANY DEMOLITION/CONSTRUCTION.
 - BUILDINGS BEING DEMOLISHED. WHERE A BUILDING IS BEING DEMOLISHED AND A STANDPIPE IS EXISTING WITHIN SUCH A BUILDING, SUCH STANDPIPE SHALL BE MAINTAINED IN AN OPERABLE CONDITION SO AS TO BE AVAILABLE FOR USE BY THE FIRE DEPARTMENT. SUCH STANDPIPE SHALL BE DEMOLISHED WITH THE BUILDING BUT SHALL NOT BE DEMOLISHED MORE THAN ONE FLOOR BELOW THE FLOOR BEING DEMOLISHED.

- GENERAL NOTES:**
- NO FLOODPLAIN EXISTS ON THE SUBJECT SITE PER FEMA FLOODPLAIN MAP # 51003C0269D DATED FEBRUARY 4, 2005.
 - NO STREAM BUFFER EXISTS ON THE SUBJECT PROPERTY.
 - BEFORE BEGINNING SITE WORK, THE CONTRACTOR SHALL INVESTIGATE AND VERIFY THE EXISTENCE AND LOCATION OF UNDERGROUND UTILITIES, MECHANICAL AND ELECTRICAL SYSTEMS, AND OTHER CONSTRUCTION AFFECTING THE WORK. BEFORE CONSTRUCTION THE CONTRACTOR SHALL VERIFY THE LOCATION AND INVERT ELEVATIONS AT POINTS OF CONNECTION OF SANITARY SEWER, STORM SEWER, AND WATER-SERVICE PIPING; UNDERGROUND ELECTRICAL SERVICES, AND OTHER UTILITIES. THE CONTRACTOR SHALL FURNISH LOCATION DATA FOR WORK RELATED TO PROJECT THAT MUST BE PERFORMED BY PUBLIC UTILITIES SERVING THE PROJECT SITE.
 - ALL WATER AND SANITARY SEWER LATERALS SHALL BE IDENTIFIED BY THE CONTRACTOR AND ABANDONED BACK TO THE MAIN WATER LINE AND SANITARY SEWER LINES. NEW SERVICES SHALL BE INSTALLED FOR THE PROPOSED BUILDINGS.
 - CONTRACTOR SHALL VERIFY SIZE, TYPE & LOCATION OF EXISTING WATER LINE IN FRANKLIN STREET.
 - THE MISS UTILITY DESIGN TICKET NUMBER FOR THIS PROJECT WAS PREVIOUSLY PROCESSED WITH THE APPROVED PACE CENTER FINAL SITE PLANS. THE TICKET NUMBERS ARE #01549 B219201116-00B & #02000 B219201116-00B FOR MONTEBELLO CIRCLE AND #01537 B219201109-00B & #02903 B219201109-00B FOR JEFFERSON PARK AVENUE.

- FIRE DEPARTMENT CONSTRUCTION & DEMOLITION NOTES:**
- SMOKING TO BE ALLOWED IN ONLY DESIGNATED SPACES WITH PROPER RECEPTACLES.
 - WASTE DISPOSAL OF COMBUSTIBLE DEBRIS SHALL BE REMOVED FROM THE BUILDING AT THE END OF EACH WORKDAY.
 - ACCESS TO THE BUILDING DURING DEMOLITION AND CONSTRUCTION SHALL BE MAINTAINED.
 - OPERATIONS INVOLVING THE USE OF CUTTING AND WELDING SHALL BE DONE IN ACCORDANCE WITH CHAPTER 26, OF THE INTERNATIONAL FIRE CODE, ADDRESSING WELDING AND HOTWORK OPERATIONS.
 - FIRE EXTINGUISHERS SHALL BE PROVIDED WITH NOT LESS THAN ONE APPROVED PORTABLE FIRE EXTINGUISHER AT EACH STAIRWAY ON ALL FLOOR LEVELS WHERE COMBUSTIBLE MATERIALS HAVE ACCUMULATED.
 - REQUIRED VEHICLE ACCESS FOR FIRE FIGHTING SHALL BE PROVIDED TO ALL CONSTRUCTION OR DEMOLITION SITES. VEHICLE ACCESS SHALL BE PROVIDED TO WITHIN 100 FEET OF TEMPORARY OR PERMANENT FIRE DEPARTMENT CONNECTIONS. VEHICLE ACCESS SHALL BE PROVIDED BY EITHER TEMPORARY OR PERMANENT ROADS, CAPABLE OF SUPPORTING VEHICLE LOADING UNDER ALL WEATHER CONDITIONS. VEHICLE ACCESS SHALL BE MAINTAINED UNTIL PERMANENT FIRE APPARATUS ACCESS ROADS ARE AVAILABLE.



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CONTRACTOR TO USE EXTREME CARE AND CAUTION AS NOT TO DAMAGE ANY TREES SCHEDULED TO REMAIN OUTSIDE LIMITS OF CONSTRUCTION. PROPERTY LINE SERVES AS LIMITS OF CONSTRUCTION.

NO CONSTRUCTION EQUIPMENT OR STORAGE SHALL OCCUR WITHIN DRIFLINE OF EXISTING TREES. PRIOR TO MOBILIZATION CONTRACTOR SHALL CONTACT LANDSCAPE ARCHITECT (LA) TO DISCUSS TREE PROTECTION MEASURES. ALL TREE PROTECTION MEASURES SHALL BE APPROVED BY LA AND/OR TREE ARBORIST BEFORE ANY CONSTRUCTION ACTIVITIES TAKE PLACE ON-SITE. CONTRACTOR TO MONITOR TREES FOR STRESS AND/OR DAMAGE AND ADVISE LA AND TREE ARBORIST IF ANY OCCUR.

CONTRACTOR TO NOTIFY LANDSCAPE ARCHITECT AND/OR TREE ARBORIST 48-HOURS IN ADVANCE OF ANY CONSTRUCTION ACTIVITY NEEDED WITHIN ANY TREE PROTECTION MEASURE. ALL TREE PROTECTION MEASURES SHALL BE REPLACED IN ORIGINAL LOCATION ONCE WORK HAS BEEN COMPLETED. NO WORK SHALL BE DONE WITHIN DRIFLINE OF EXISTING TREES UNLESS APPROVED BY TREE ARBORIST OR LANDSCAPE ARCHITECT PRIOR TO WORK.

ALL WORK TO BE PERFORMED BY THE CONTRACTOR WITHIN THE DRIFLINE OF ANY EXISTING TREE OR TREE PROTECTION AREA SHALL BE DONE IN A MANNER SENSITIVE TO ENSURING NO DAMAGE WILL BE DONE TO THE EXISTING TREES. THE PREFERRED METHOD FOR GRADING SMALL AREAS WITHIN THE DRIFLINE SHALL BE DONE BY HAND. LARGER AREAS TO BE GRADED MAY BE DONE WITH A SMALL BOBCAT/TRACTOR-HOE. CONTRACTOR TO DISCUSS METHODS OF GRADING WORK WITH LANDSCAPE ARCHITECT AND TREE ARBORIST PRIOR TO COMMENCING ANY SUCH WORK WITHIN DESIGNATED TREE PROTECTION AREAS OR WITHIN EXISTING DRIFLINES.

ALL PLANTS HAVING A QUANTITY GREATER THAN ONE(1) SHALL BE MATCHED AND SUPPLIED FROM THE SAME SOURCE (PER SPECIES).

CONTACT LANDSCAPE ARCHITECT AT THE TIME OF PLANT MATERIAL DELIVERY, BEFORE ANY SUBSTITUTIONS OR CHANGES, IF SCHEDULED TYPES ARE UNAVAILABLE, AND FOLLOWING INSTALLATION. ALL PLANT SUBSTITUTIONS SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO PLACEMENT OF ORDERS.

LANDSCAPE ARCHITECT SHALL INSPECT AND APPROVE ALL PLANT MATERIAL AT TIME OF DELIVERY AS WELL AS AFTER INITIAL PLACEMENT PRIOR TO PLANTING. CONTRACTOR TO NOTIFY LANDSCAPE ARCHITECT 48-HOURS PRIOR TO DELIVERY.

PLANT LOCATIONS TO BE REEVALUATED AND REVISED, IF NECESSARY, AFTER FINISHED GRADING.

MULCH IN PLANTERS AND PLANTING BEDS TO BE CLEAN AND FREE FROM PEST AND DISEASES. MULCH SHALL BE APPLIED TO A 2-INCH DEPTH. MULCH RINGS 24-INCHES MIN. IN DIAMETER ARE TO BE PLACED AROUND ALL TREES NOT LOCATED IN PLANTING BEDS. MULCH TO BE DOUBLE-SHREDED HARDWOOD.

CONTRACTOR TO VERIFY ALL QUANTITIES BETWEEN PLAN AND PLANT LIST AND REPORT ANY DISCREPANCIES TO THE LANDSCAPE ARCHITECT IMMEDIATELY PRIOR TO ORDERING.

ALL STREET TREES WITHIN THE PUBLIC RIGHT OF WAY SHALL BE MAINTAINED BY THE HOMEOWNERS ASSOCIATION.

- SITE NOTES:**
- ALL SIDEWALKS AND WALKWAYS SHALL HAVE A MINIMUM CLEAR WIDTH OF 5'
 - ALL WALKWAY CROSSINGS SHALL MEET MINIMUM ADA ACCESSIBILITY STANDARDS.
 - CONTRACTOR SHALL OBTAIN A TEMPORARY STREET CLOSURE PERMIT FOR CLOSURE OF SIDEWALKS, PARKING SPACES & ROADWAYS SUBJECT TO APPROVAL BY THE CITY TRAFFIC ENGINEER PRIOR TO CONSTRUCTION IN THESE EXISTING AREAS.
 - ALL SIGNING & PAVEMENT MARKINGS SHALL BE INSTALLED CONSISTENT WITH MUTCD STANDARDS.
 - RAMP OVER 30" IN ELEVATION CHANGE REQUIRE HANDRAILS.

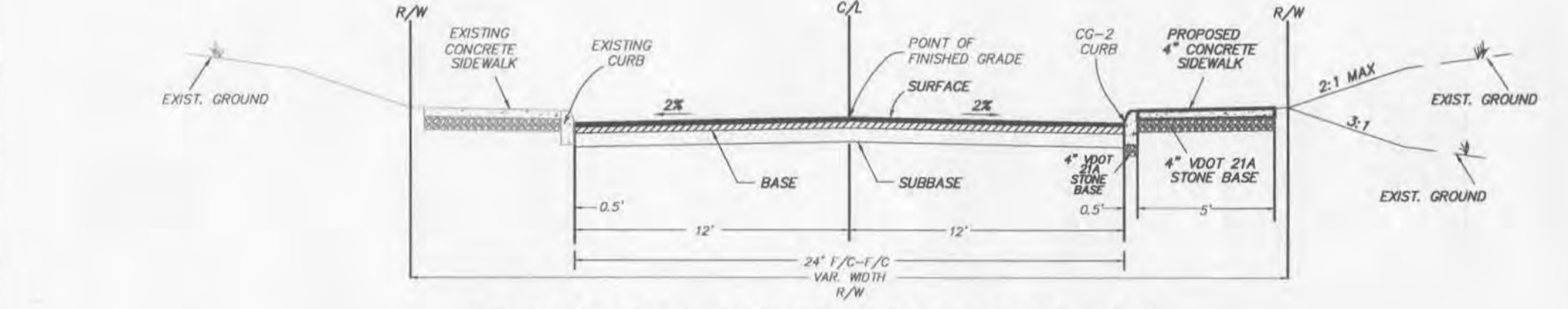
- LANDSCAPING NOTES:**
- ALL DUMPSTERS SHALL BE SCREENED WITH AN ENCLOSURE AT A MINIMUM HEIGHT OF ONE (1) FOOT ABOVE THE HEIGHT OF THE DUMPSTER AND WITH A MINIMUM INSIDE CLEARANCE AT THE OPENING OF TWELVE (12) FEET.
 - ALL PLANTINGS SHALL HAVE A MINIMUM HEIGHT OF EIGHTEEN (18) INCHES WHEN PLANTED. PLANTINGS SHALL BE EVENLY SPACED IN A ROW, AT INTERVALS SUFFICIENT TO ALLOW FOR THEIR HEALTHY GROWTH AND DEVELOPMENT.
 - TOTAL JEFFERSON PARK AVENUE ROAD FRONTAGE = 200'. TOTAL STREET TREES REQUIRED: 5 STREET TREES PROVIDED: 5
 - TOTAL MONTEBELLO CIRCLE ROAD FRONTAGE = 210'. TOTAL STREET TREES REQUIRED: 5 STREET TREES PROVIDED: 5
 - NOTE, NO TREES TO BE MAINTAINED BY THE CITY OF CHARLOTTESVILLE
 - LARGE STREET TREES SHALL BE PLANTED WITHIN A PLANTING STRIP WITH A MINIMUM OF 8' WIDE, AND SOIL VOLUME OF 900 CF PER TREE, WITH A SPACING OF 30' MIN.

TMP: 160034000
 OWNER: PAYNE, CARSON
 ADDRESS: 122 MONTEBELLO CIRCLE
 CHARLOTTESVILLE, VA 22903
 ZONING: R-3/RESIDENTIAL
 DB/PG: 2014/63

TMP: 160014100
 OWNER: DUNOVA, LLC
 ADDRESS: 355 MALLARD LANE
 EARLYSVILLE, VA 22936
 ZONING: R-3/RESIDENTIAL
 DB/PG: 900/126

TMP: 160015000
 OWNER: WEITZNER PROPERTIES, LLC
 ADDRESS: 501 SLATERS LANE, APT 610
 ALEXANDRIA, VA 22314
 ZONING: R-3/RESIDENTIAL
 DB/PG: 739/44

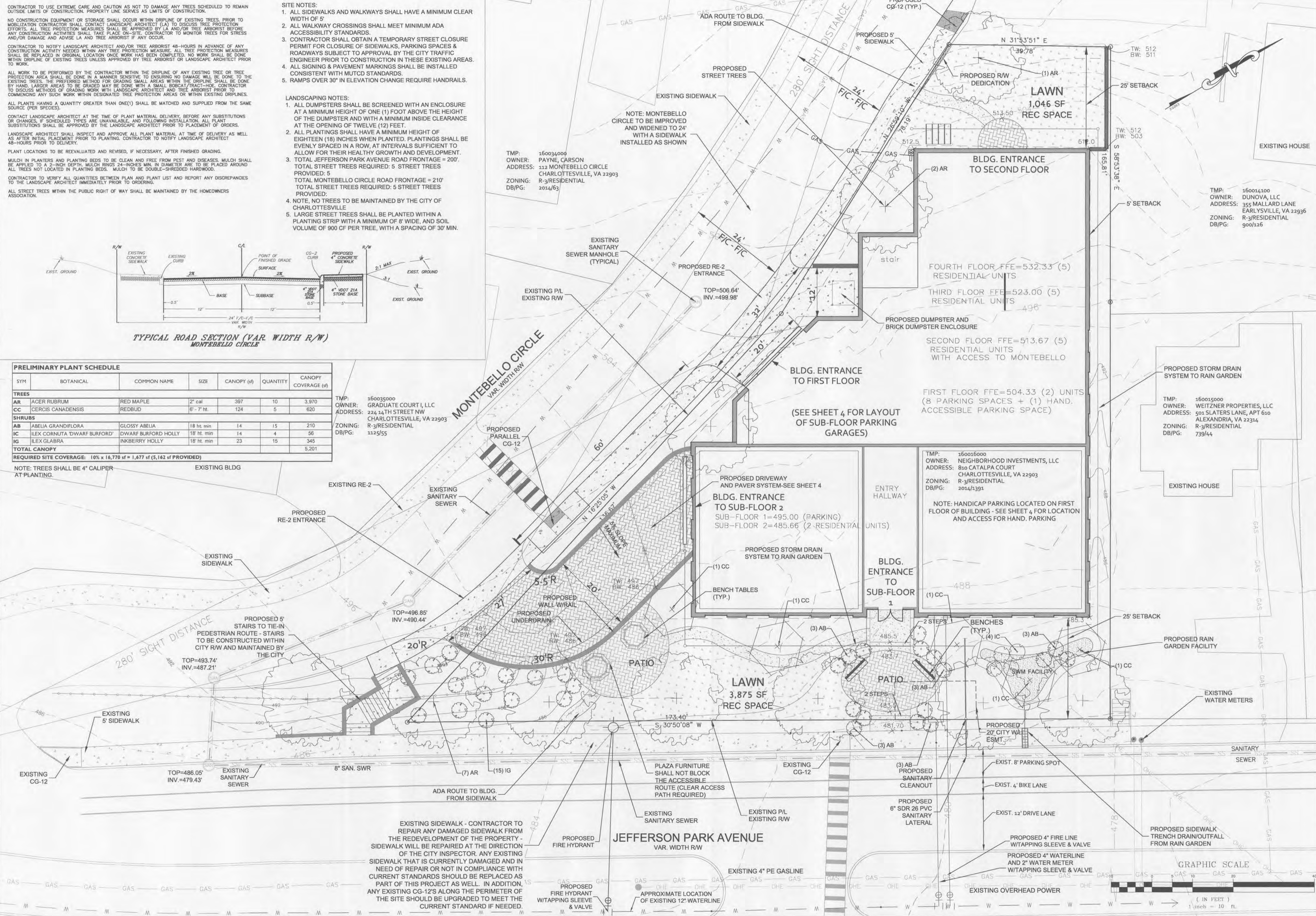
TMP: 160016000
 OWNER: NEIGHBORHOOD INVESTMENTS, LLC
 ADDRESS: 810 CATALPA COURT
 CHARLOTTESVILLE, VA 22903
 ZONING: R-3/RESIDENTIAL
 DB/PG: 2014/1391



PRELIMINARY PLANT SCHEDULE

SYM	BOTANICAL	COMMON NAME	SIZE	CANOPY (sq ft)	QUANTITY	CANOPY COVERAGE (sq ft)
TREES						
AR	ACER RUBRUM	RED MAPLE	2" cal	397	10	3,970
CC	CERCIS CANADENSIS	REDBUD	6"-7" ht.	124	5	620
SHRUBS						
AB	ABELIA GRANDIFLORA	GLOSSY ABELIA	18 ht. min	14	15	210
IC	ILEX CORNUTA 'DWARF BURFORD'	DWARF BURFORD HOLLY	18 ht. min	14	4	56
IG	ILEX GLABRA	INKBERRY HOLLY	18 ht. min	23	15	345
TOTAL CANOPY						
REQUIRED SITE COVERAGE: 10% x 16,770 sq ft = 1,677 sq ft (5,162 sq ft PROVIDED)						

NOTE: TREES SHALL BE 4" CALIPER AT PLANTING.



REVISIONS

REVISION DESCRIPTION	DATE
INITIAL SUBMITTAL	2/24/15
REVISED PER CLIENT CHANGES AND CITY COMMENTS	4/21/15
REVISED PER CITY COMMENTS	6/12/15

COLLINS ENGINEERING

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1725 JPA APARTMENTS PRELIMINARY SITE PLAN

SITE & LANDSCAPING PLAN



PROJECT: 102045
 JOB NO.: 102045
 SCALE: 1" = 10'
 SHEET NO.: 3

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GENERAL NOTES:

- UTILITIES
1. ANY DAMAGE TO EXISTING UTILITIES CAUSED BY CONTRACTOR OR ITS SUBCONTRACTORS SHALL BE CONTRACTOR'S SOLE RESPONSIBILITY AND REPAIRED AT CONTRACTOR'S EXPENSE.
2. THE CONTRACT DOCUMENTS DO NOT GUARANTEE THE EXISTENCE, NON-EXISTENCE OR LOCATION OF UTILITIES...

CONCRETE AND ASPHALT

- 9. ALL FORMS SHALL BE INSPECTED BY THE ENGINEER BEFORE ANY CONCRETE IS PLACED. THE ENGINEER MAY REQUIRE CONTRACTOR, AT NO ADDITIONAL COST, TO REMOVE AND REPLACE CONCRETE PLACED PRIOR TO OR WITHOUT SUCH INSPECTION.
10. ALL MATERIAL INSIDE FORMS SHALL BE CLEAN AND FREE OF ALL ROCKS AND OTHER LOOSE DEBRIS...

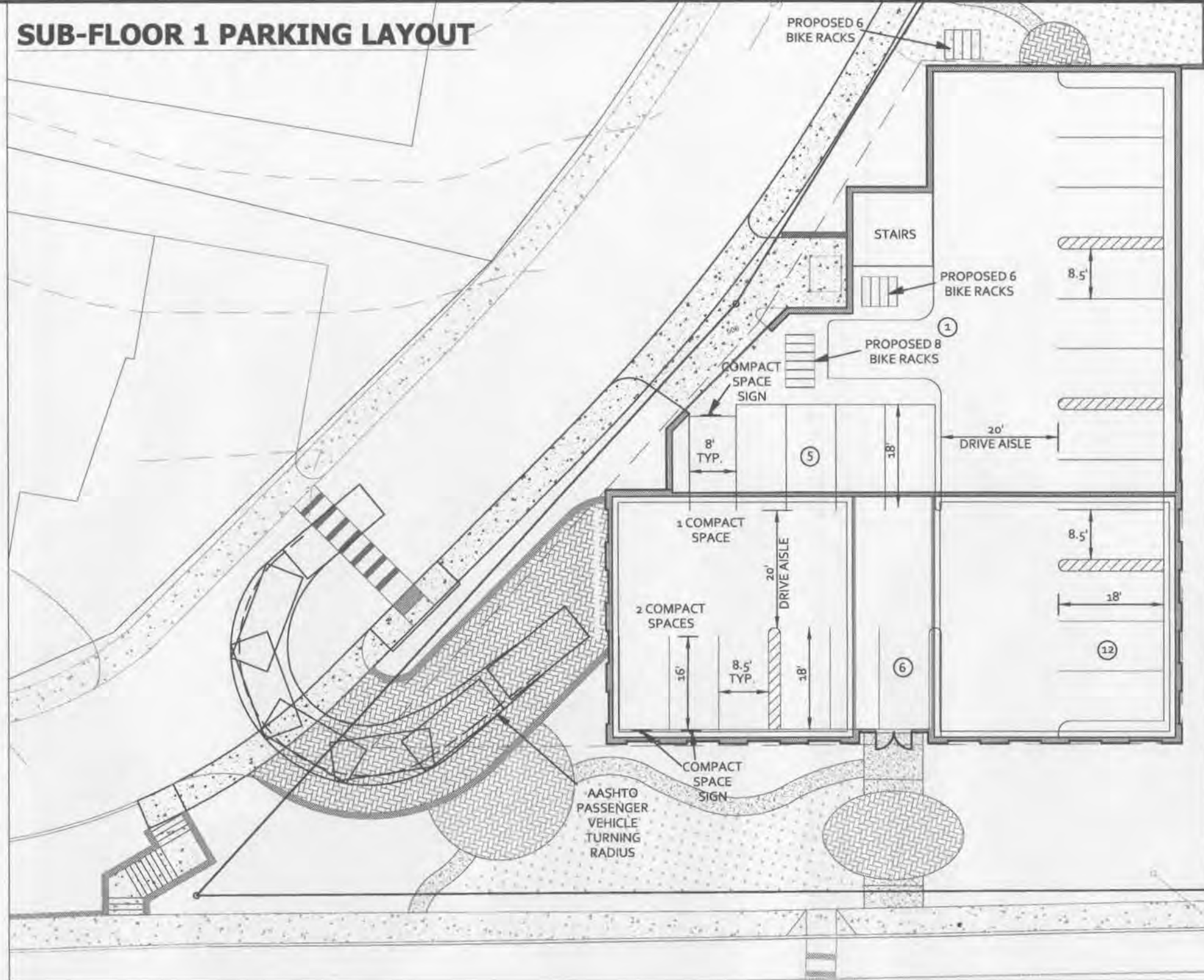
DRAINAGE

- 20. CONTRACTOR SHALL EXERCISE CARE, ESPECIALLY AT INTERSECTIONS AND GUTTER LINES, TO PROVIDE POSITIVE DRAINAGE. ANY AREAS WHERE WATER IS IMPOUNDED SHALL BE CORRECTED BY CONTRACTOR AT NO ADDITIONAL COST.
21. CONTRACTOR SHALL MAINTAIN EXISTING STREAMS, DITCHES, DRAINAGE STRUCTURES, CULVERTS AND FLOWS AT ALL TIMES DURING THE WORK...

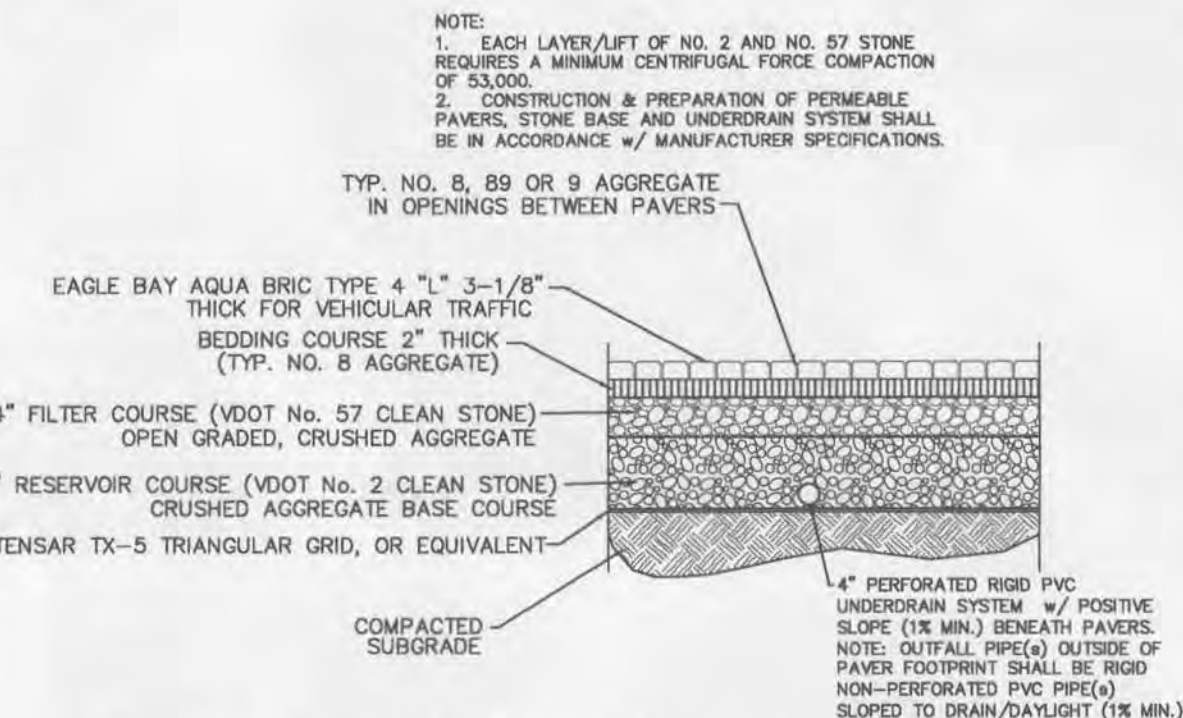
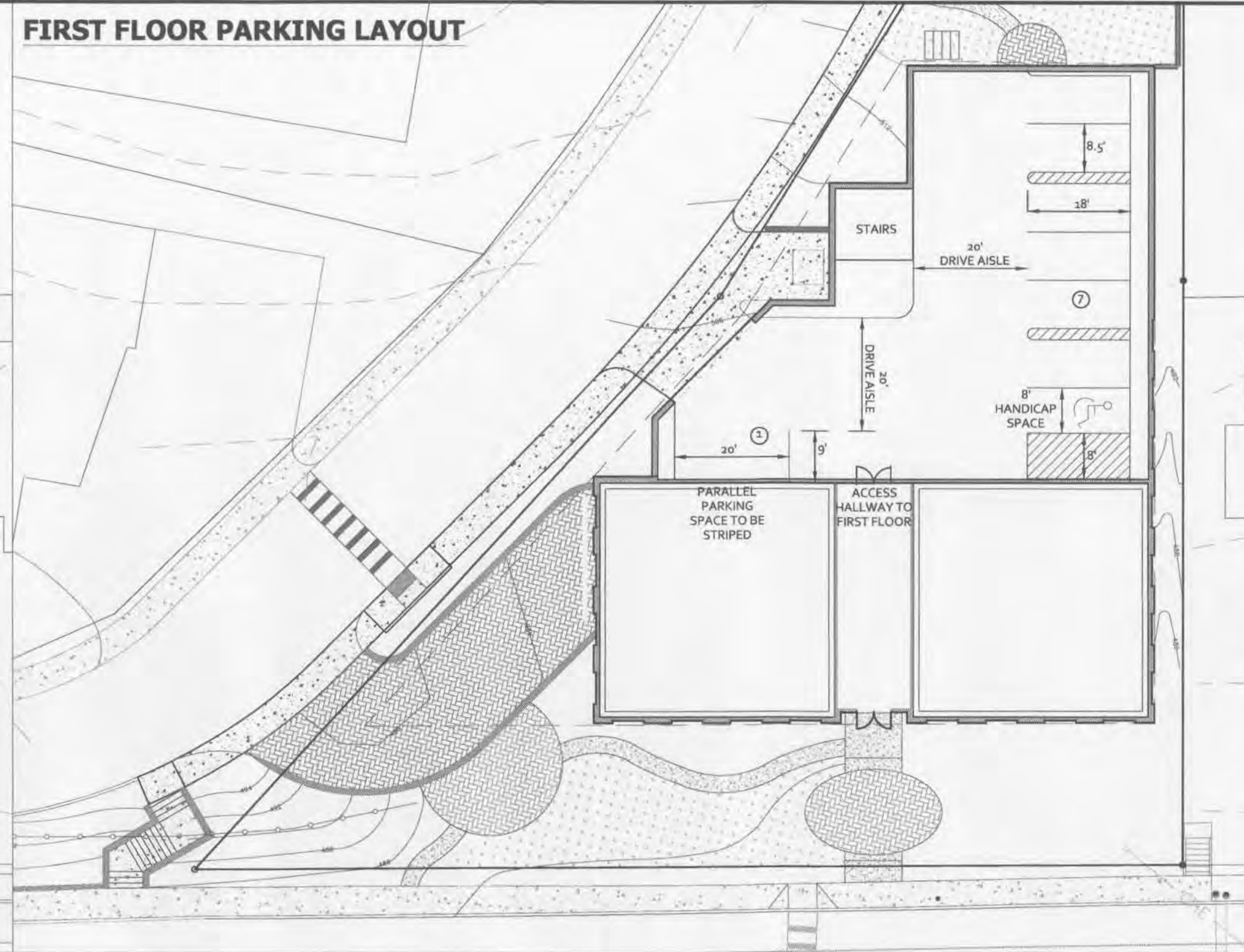
REQUIREMENTS FOR THE INSTALLATION OF GAS MAINS, SERVICES, AND METERS GAS UNIT:

- GAS MAINS WILL BE INSTALLED WHEN THE FOLLOWING CONDITIONS ARE MET IN THE CONSTRUCTION AREA:
1. GRADE IS WITHIN 6 INCHES OF FINAL GRADE OR BASE GRADE IN ROADWAYS.
2. CURB AND GUTTER MUST BE INSTALLED IF GAS MAIN IS GOING TO BE INSTALLED IN OR NEAR THE ROADWAY.

SUB-FLOOR 1 PARKING LAYOUT



FIRST FLOOR PARKING LAYOUT



PERMEABLE PAVERS PAVEMENT DESIGN

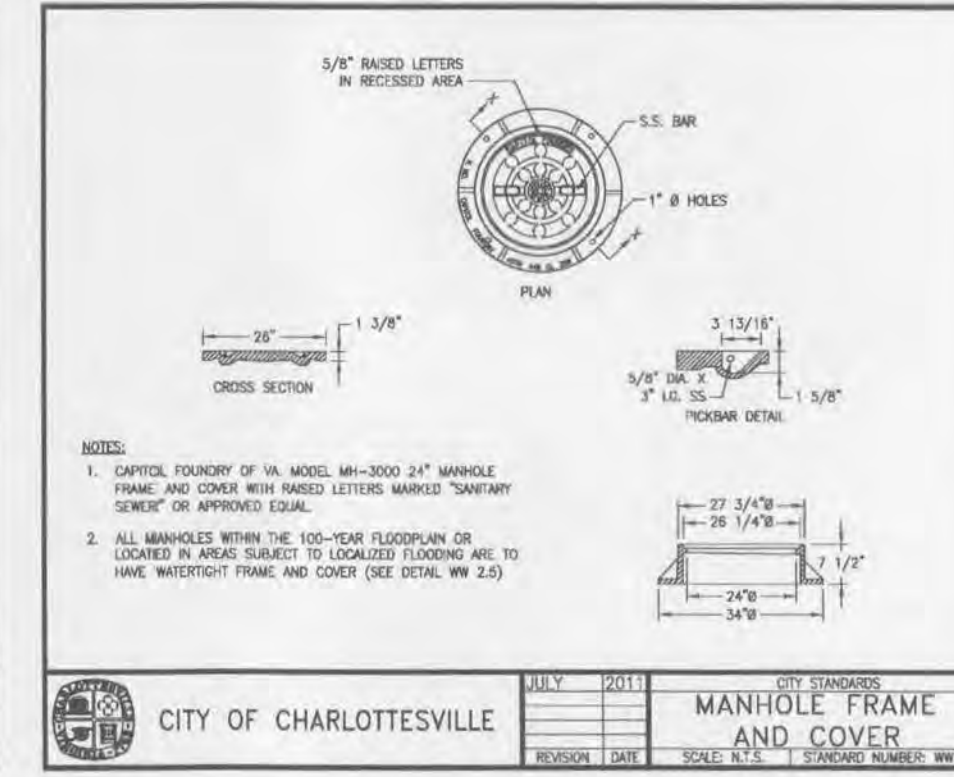
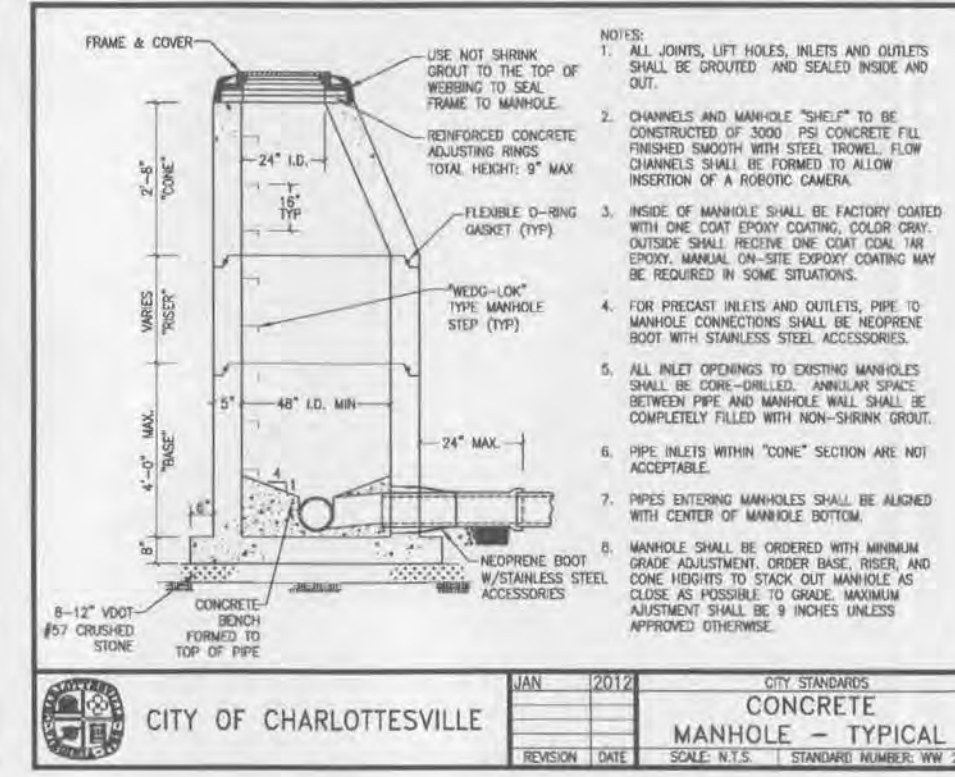
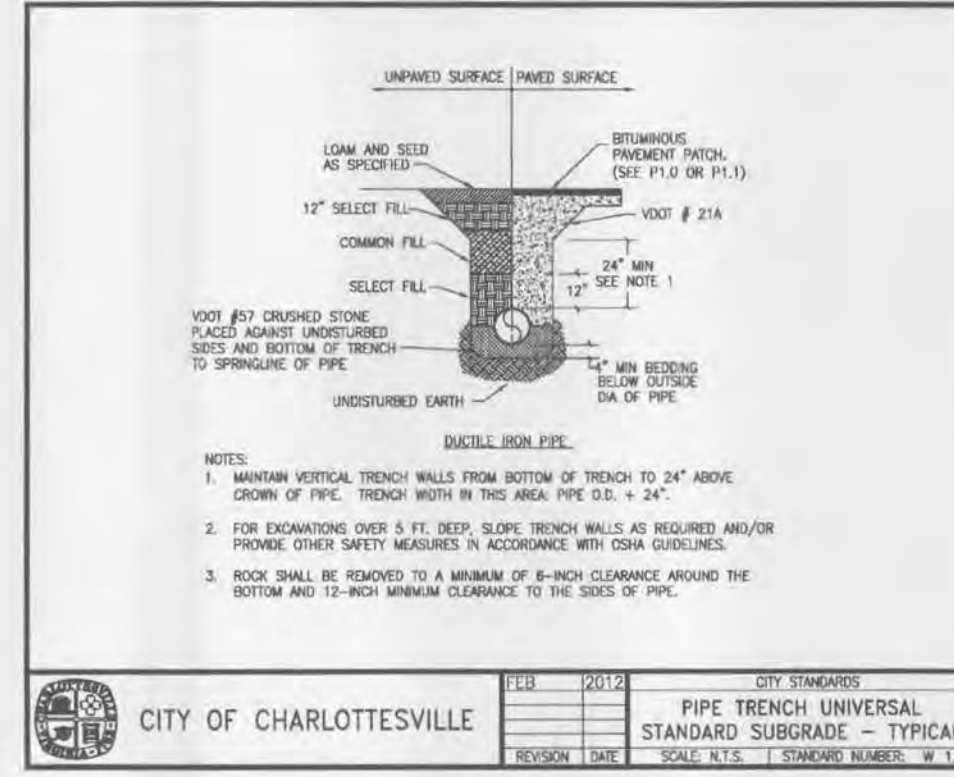
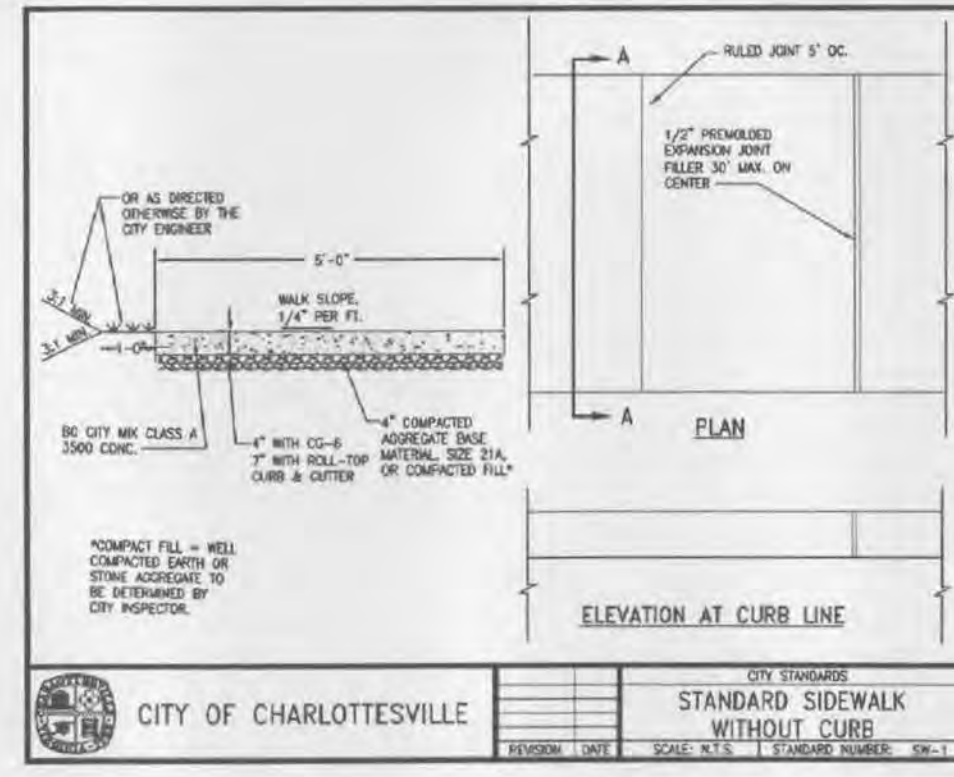
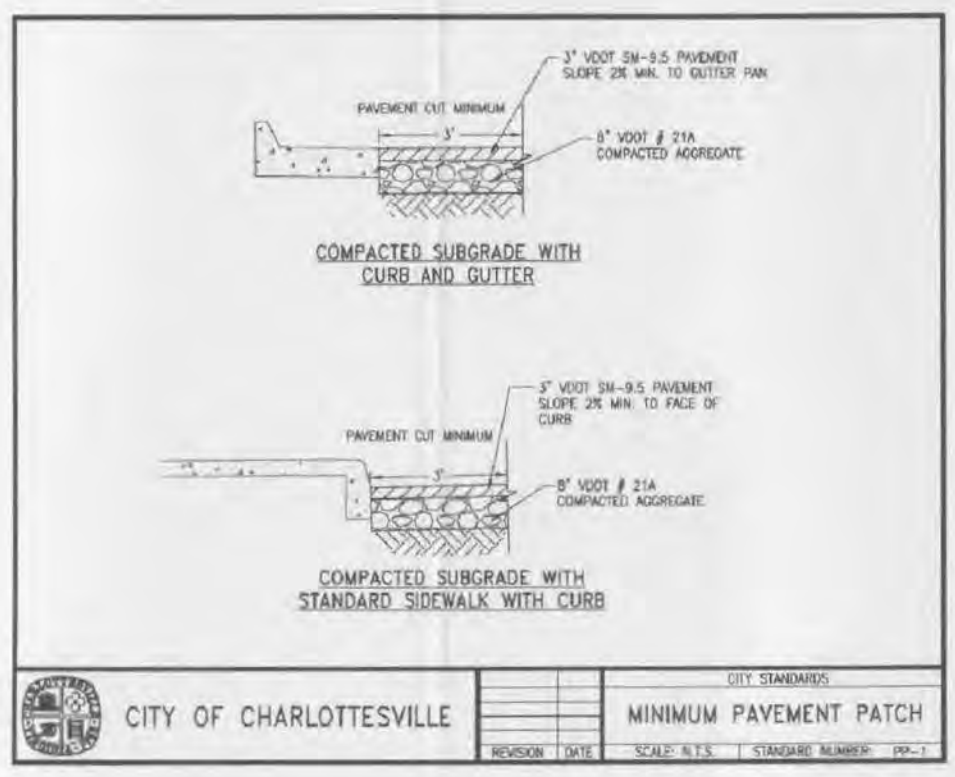
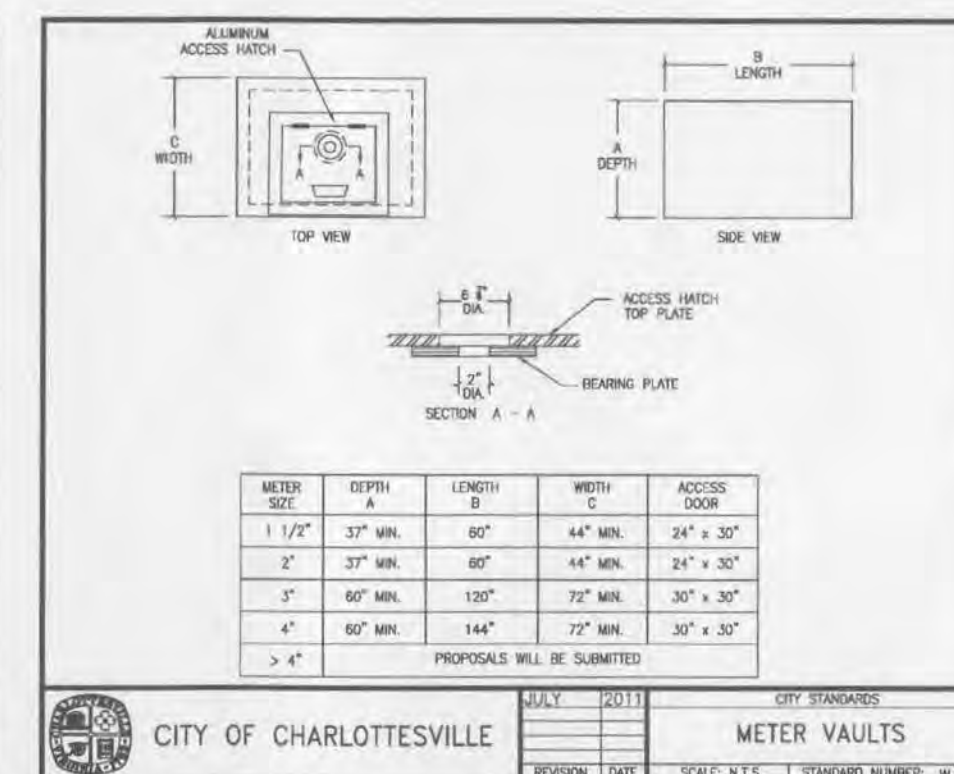
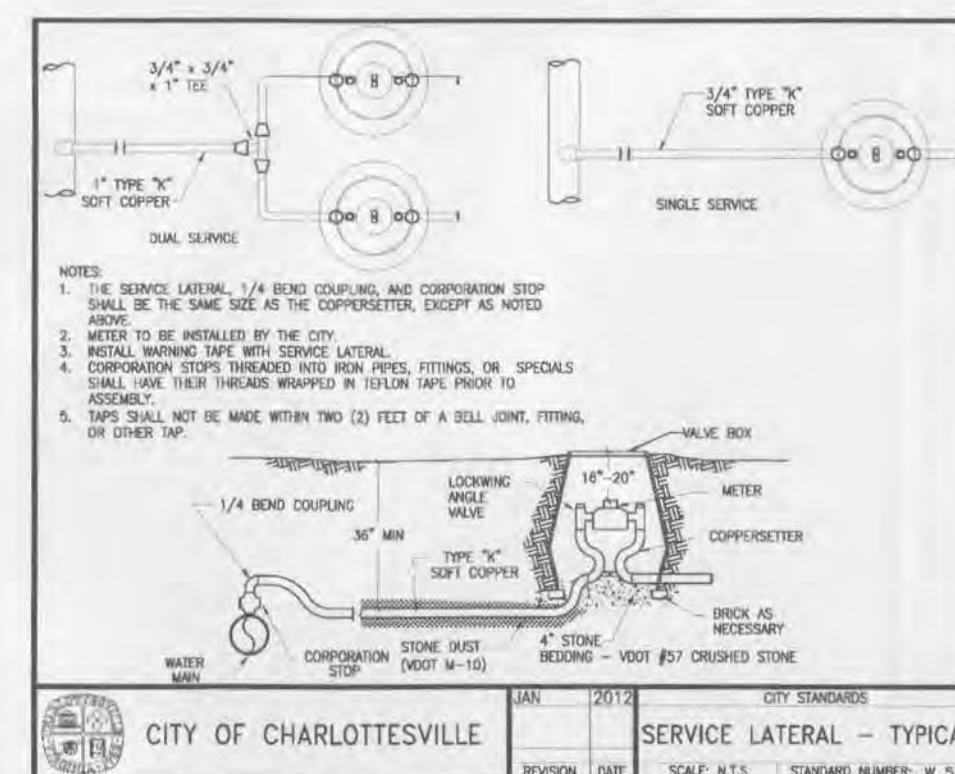
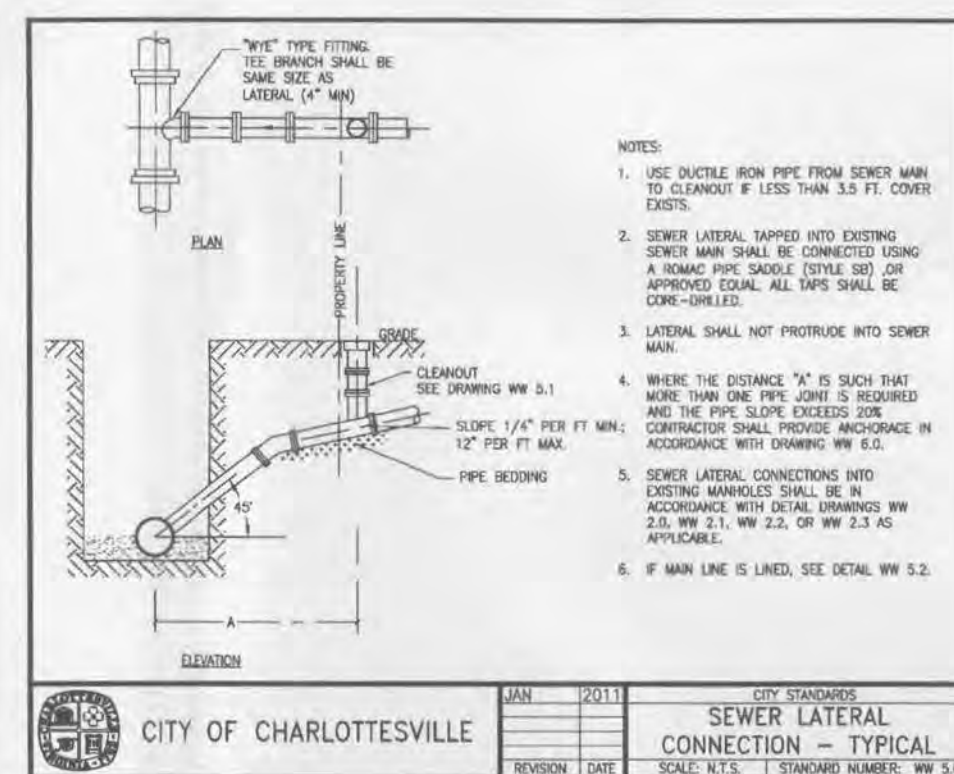
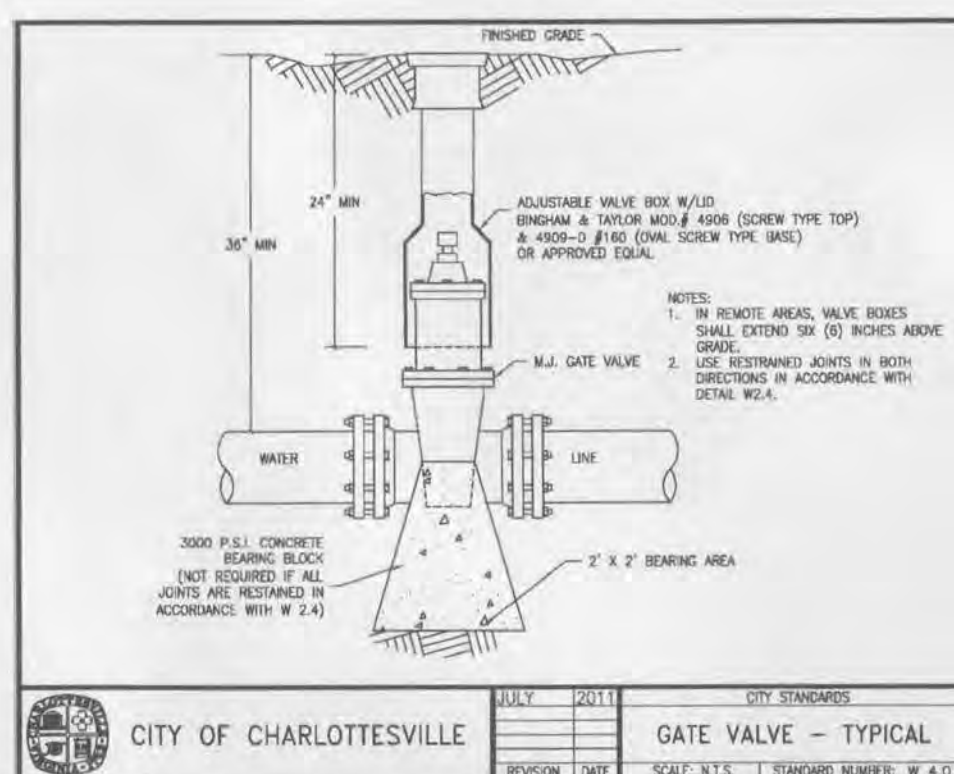
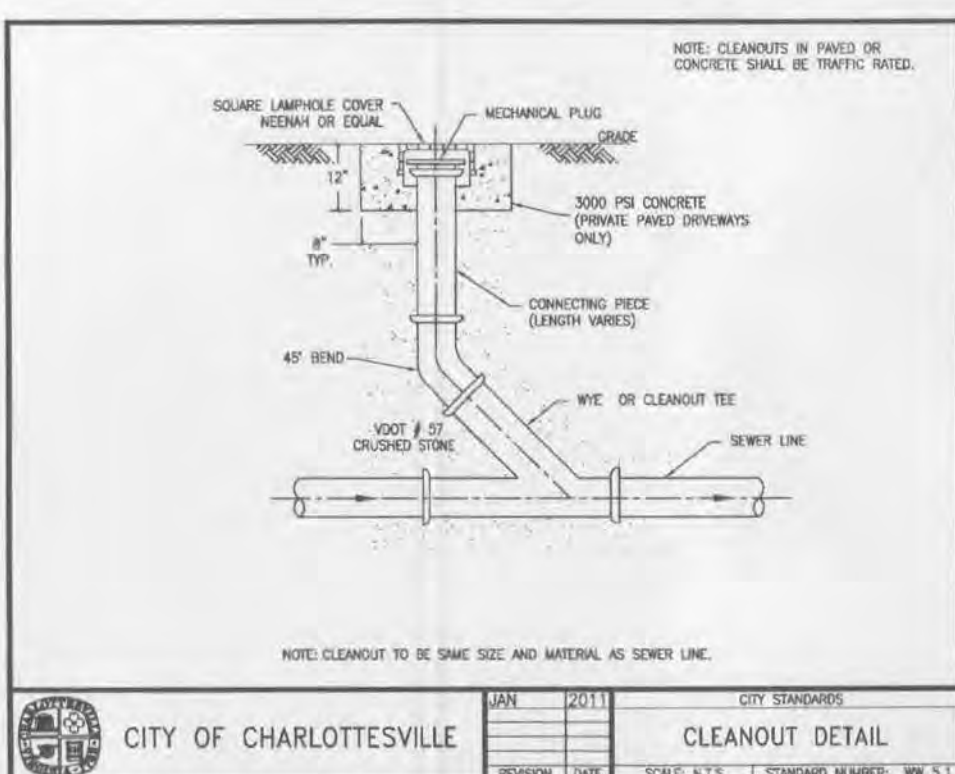
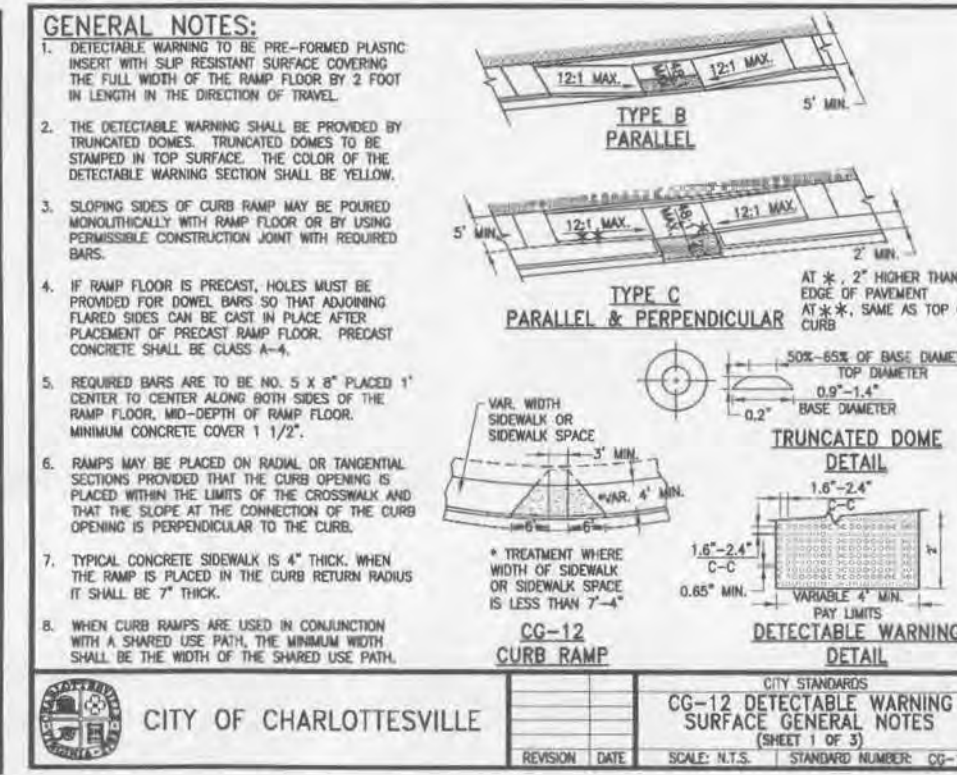
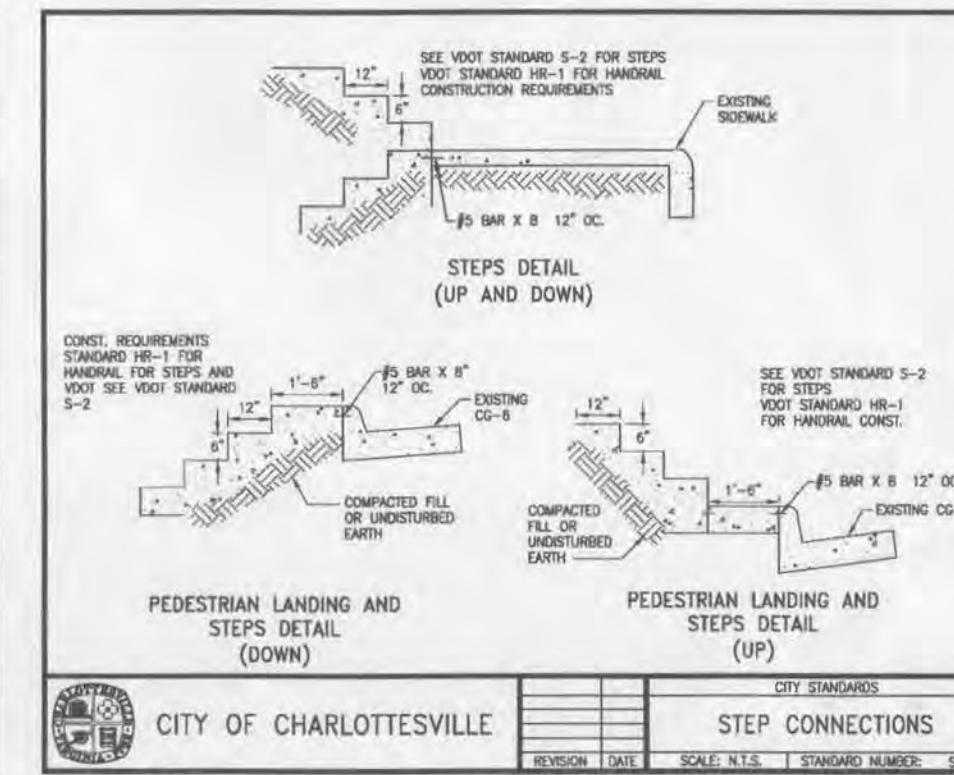
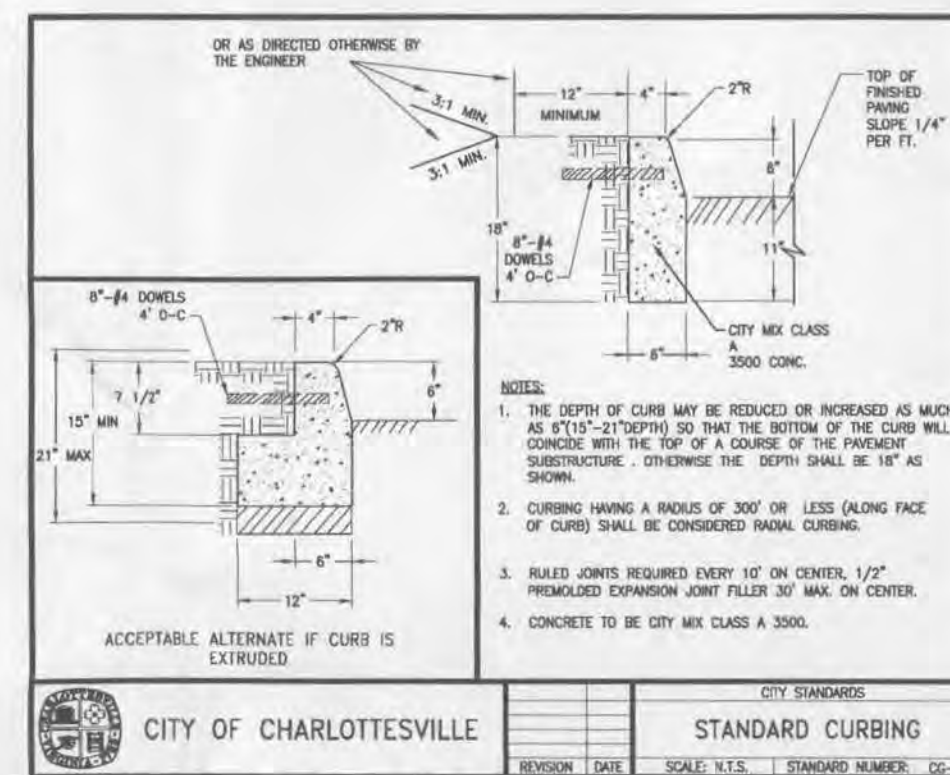


Table with 4 columns: REVISIONS, REVISION DESCRIPTION, INITIAL SUBMITTAL, REVISED PER CLIENT CHANGES AND CITY COMMENTS, DATE.

Large vertical text block: COLLINS ENGINEERING, 1725 JPA APARTMENTS PRELIMINARY SITE PLAN, NOTES & DETAILS, 200 GARRETT STREET, SUITE K - CHARLOTTESVILLE, VA 22902 - 434.293.3719.

These plans and associated documents are the exclusive property of COLLINS ENGINEERING and may not be reproduced in whole or in part and shall not be used for any purpose whatsoever...

CITY OF CHARLOTTESVILLE
"A World Class City"

Neighborhood Development Services
610 East Market Street
Charlottesville, VA 22902
Telephone 434-970-3182
Fax 434-970-3359
www.charlottesville.org



July 2, 2015

Scott Collins, PE
Collins Engineering
200 Garrett Street, Suite K
Charlottesville, VA 22902

RE: 1725 JPA Apartments Preliminary Site Plan

Dear Scott,

The above referenced site plan was submitted to this office for review on June 12, 2015. Please find below a list of revisions that are necessary as the plan moves from preliminary to final. The revisions outlined in this letter may be addressed during final site plan submittal. Please submit 3 copies and a PDF file of this project to be placed on the August 11, 2015 Planning Commission agenda. All materials need to be received by July 21, 2015. If you have questions, please contact me at 434-970-3636 or alfelem@charlottesville.org.

Sincerely yours,

Matthew Alfele
City Planner
CC: via email (Scott Collins, PE) scott@collins-engineering.com

The following items need to be addressed in the revised site plan: Be advised that any major changes to the site plan may result in new comments.

Engineering Division: *Marty Silman*

1. The proposed plan for Stormwater is to discharge to the street; however the regulations are clear that discharge should occur to an adequate channel. We do not believe the road qualifies as an adequate channel.

Traffic Engineer: *Christina Fisher*

1. The stairs leading to the public sidewalk on Montebello Circle is not ADA accessible from JPA. Extend the sidewalk down to existing on JPA.

Planning Division: *Matt Alfele*

Sheet 1 / General

1. Include SUP information and conditions. The SUP conditions listed on the cover are not the ones City Council passed. City Council made a change to the language in condition (4). See attached resolution.
2. Change (#5) The SUP that was approved by City Council caps residential density at 49.875 DUA. See attached resolution.
3. Change (#12) Change to reflect that the max gross density allowed under the SUP is 49.875 DUA, not a range.

Sheet 3

1. Update the caliper of the Red Maples to reflect the conditions of the SUP.

Be advised that major changes to the development may warrant additional comments not expressed in this review.

Urban Design: *Carrie Rainey*

1. It appears the wall along the parking garage entrance will be 1-foot higher than the driveway surface. Please confirm this is accurate, or provide correct wall height on the driveway side if not. Please indicate railing height.

Bicycle and Pedestrian Coordinator: *Amanda Poncy*

1. Was an accessible route on the north/east side of Stewart Street Circle explored? For example, rather than provide the staircase near the Stewart St/JPA intersection, could a sidewalk continue along the north/east side of the road instead? Or could a ramp be provided? This would create the accessible route and the midblock crossing could be removed. It should be noted that the midblock crossing would not create an “accessible route” since there would not be a curb ramp on the other side of the marked crosswalk in the immediate term.

Public Utilities: Trip Stakem

WATER:

1. Show calculated Peak Hour water demand as well.
2. The fixture count total (399.8 wsfu) does not correlate with the demand shown (105 gpm). Please revise this number accordingly. Based on my calculations, a 2" meter is too big.

DETAILS:

1. All relevant City Utility details will be required prior to final approval. Please remove details that are not relevant to this project.

RESOLUTION
APPROVING A SPECIAL USE PERMIT
AS REQUESTED BY APPLICATION NO. SP-1500001
TO AUTHORIZE ADDITIONAL RESIDENTIAL DENSITY AND TO
MODIFY CERTAIN YARD REQUIREMENTS IN CONNECTION WITH
THE CONSTRUCTION AND ALTERATION OF A MULTIFAMILY RESIDENTIAL
DWELLING LOCATED AT 1725 JEFFERSON PARK AVENUE

WHEREAS, Neighborhood Investments, LLC, the owner of property located at 1725 Jefferson Park Avenue, acting by its duly authorized agent (“Applicant”) has submitted application SP-1500001 (“Application”) seeking approval of a special use permit authorizing additional residential density, and requesting modification of required yards, in connection with the construction of a multifamily residential dwelling at 1725 Jefferson Park Avenue, which property is identified on City Tax Map 16 as Parcel 16 (“Subject Property”), as such proposed development is depicted within a site plan submitted in connection with the Application; and,

WHEREAS, the Application seeks authorization pursuant to §34-420 of the City Code to construct a multifamily dwelling unit containing 19 dwelling units, an effective residential density of 49.875 DUA, and to modify the side yard requirement of City Code § 34-353(a) to establish a minimum 5-foot side yard requirement for the proposed development; and

WHEREAS, the Subject Property is zoned “R-3” (Multifamily residential) subject to the requirements of the City’s entrance corridor overlay district zoning regulations; and

WHEREAS, following a joint public hearing before the Planning Commission and City Council, duly advertised and held on May 12, 2015, the Planning Commission reviewed this application and determined that the proposed special use permit, under suitable regulations and safeguards set forth within a list of recommended conditions, will serve the interests of the public necessity, convenience, general welfare or good zoning practice, and will conform to the criteria generally applicable to special permits as set forth within §§ 34-156 et seq. of the City Code, and the Planning Commission has transmitted its recommendation to City Council; and

WHEREAS, this Council concurs with the Planning Commission and hereby finds and determines that, under suitable regulations and safeguards, the proposed special use permit will serve the interests of the public necessity, convenience, general welfare or good zoning practice, and will conform to the criteria generally applicable to special permits as set forth within §§ 34-156 et seq. of the City Code and will be consistent with the purpose of the multifamily residential zoning district. **NOW, THEREFORE**,

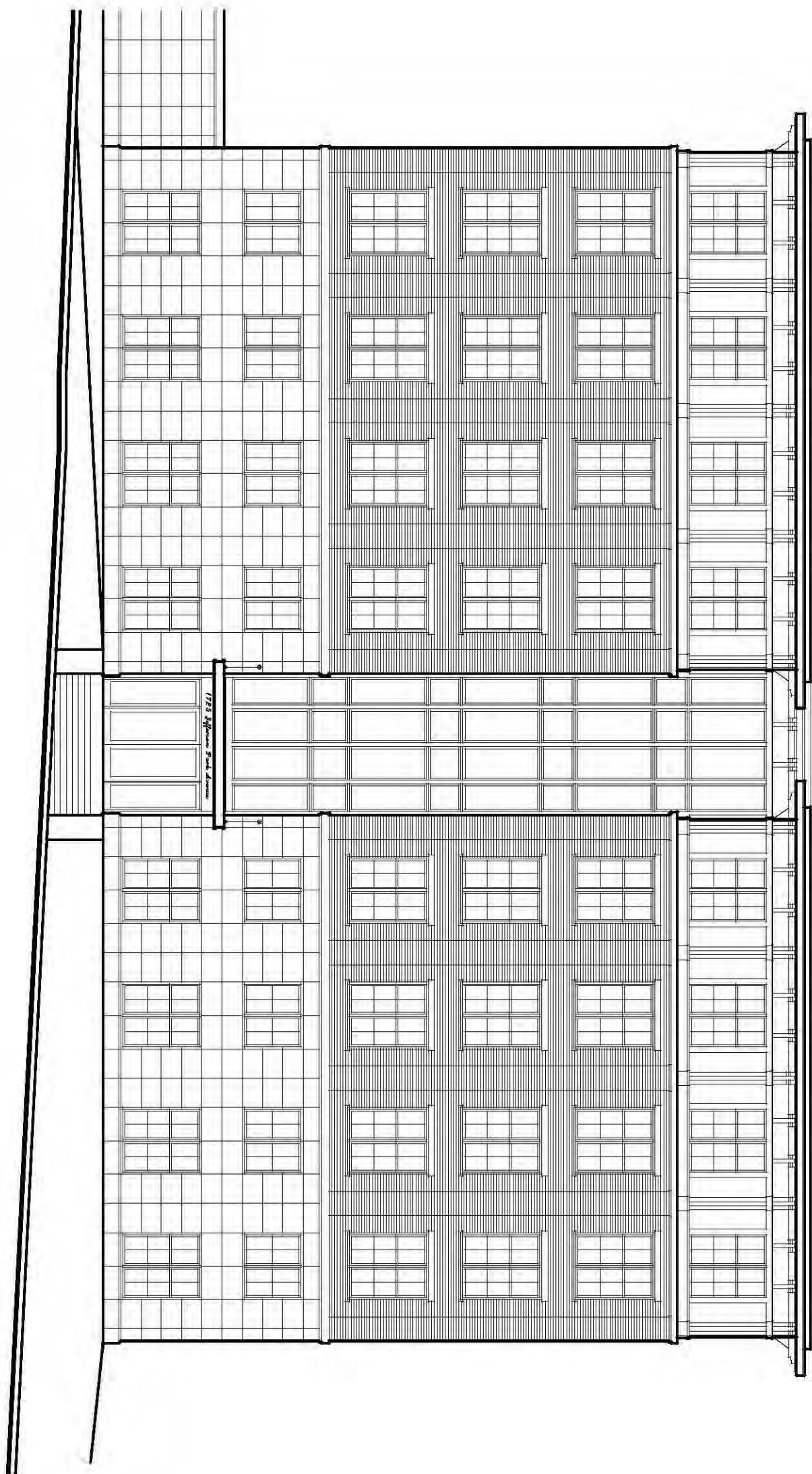
BE IT RESOLVED by the City Council of the City of Charlottesville, that a special use permit is hereby approved, to authorize: (i) a multifamily dwelling of up to 19 dwelling units to be developed on the Subject Property, and (ii) a minimum side yard requirement of 5 feet.

AND BE IT FURTHER RESOLVED that this special use permit is granted subject to the following conditions:

1. Conform to *Sec 34-881-Bicycle Storage Facilities* or the most current Bicycle Storage Facilities code at time of development.
2. The finished floor elevation (FFE) and building entrance shall be no more than (6.5) feet above the average elevation of Jefferson Park Avenue that runs in front of the property.
3. Street trees shall be required as depicted with the application materials dated April 21, 2015, submitted to the City for and in connection with SP-1500001 (“Application”) and be 4” caliper at planting.

4. The design, height, density, and other characteristics of the development shall remain essentially and substantially the same, in all material aspects, as described within the application materials dated April 21, 2015, submitted to the City for and in connection with SP-1500001 (“Application”). Except as the design details of the development may subsequently be modified to specifically comply with requirements of a certificate of appropriateness issued by the City’s Entrance Corridor Review, staff comments, or by any other provision(s) of these SUP Conditions, any change of the development that is inconsistent with the application shall require a modification of this SUP.
5. All outdoor lighting and light fixtures shall be full cut-off luminaires.
6. If the developer elects to make a contribution to the City’s Affordable Housing Fund to satisfy City Code 34-12(d)(2), no building permit shall be issued for the development until the amount of the contribution is calculated by the Director of Neighborhood Development Services, or designee, and until such contribution has been paid in full to the City. If the developer elects to provide affordable dwelling units pursuant to City Code 34-12(a) or 34-12(d)(1), then a written CAU Commitment must be submitted and approved in accordance with the regulations adopted by City Council pursuant to City Code 34-12(g) (“Regulations”), as specified within the Regulations.

1 SOUTH (FRONT) ELEVATION



3/32"=1'-0"

SUP.07

1725 JEFFERSON PARK AVENUE
SPECIAL USE PERMIT
APPLICATION PACKAGE
BUILDING ELEVATION

JOB NUMBER 1409

FILE NO. 1409-SUP-WORKFILE

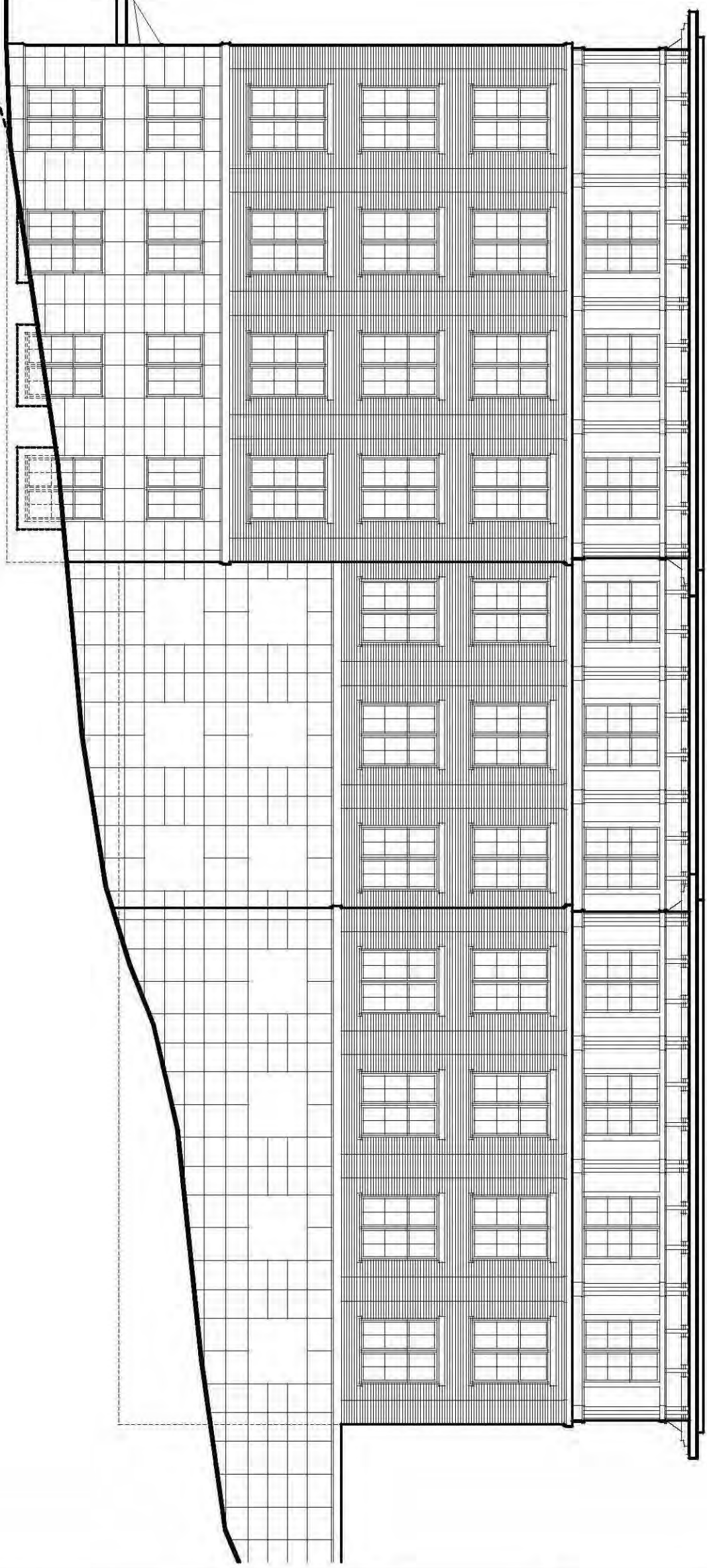
NO.	DATE	DRAWING RELEASE
1	4-21-15	SUP SUBMISSION
NO.	DATE	REVISION



**ATWOOD HENNINGSEN KESTNER
ARCHITECTS
INC.**

1108 EAST HIGH STREET
CHARLOTTESVILLE, VA 22902
PHONE (434) 971-7202 FAX (434) 295-2413

1 EAST (RIGHT SIDE) ELEVATION



0-11.56/E

SUP.08

1725 JEFFERSON PARK AVENUE
 SPECIAL USE PERMIT
 APPLICATION PACKAGE
 BUILDING ELEVATION

JOB NUMBER 1409 FILE NO. 1409-SUP-WORKFILE

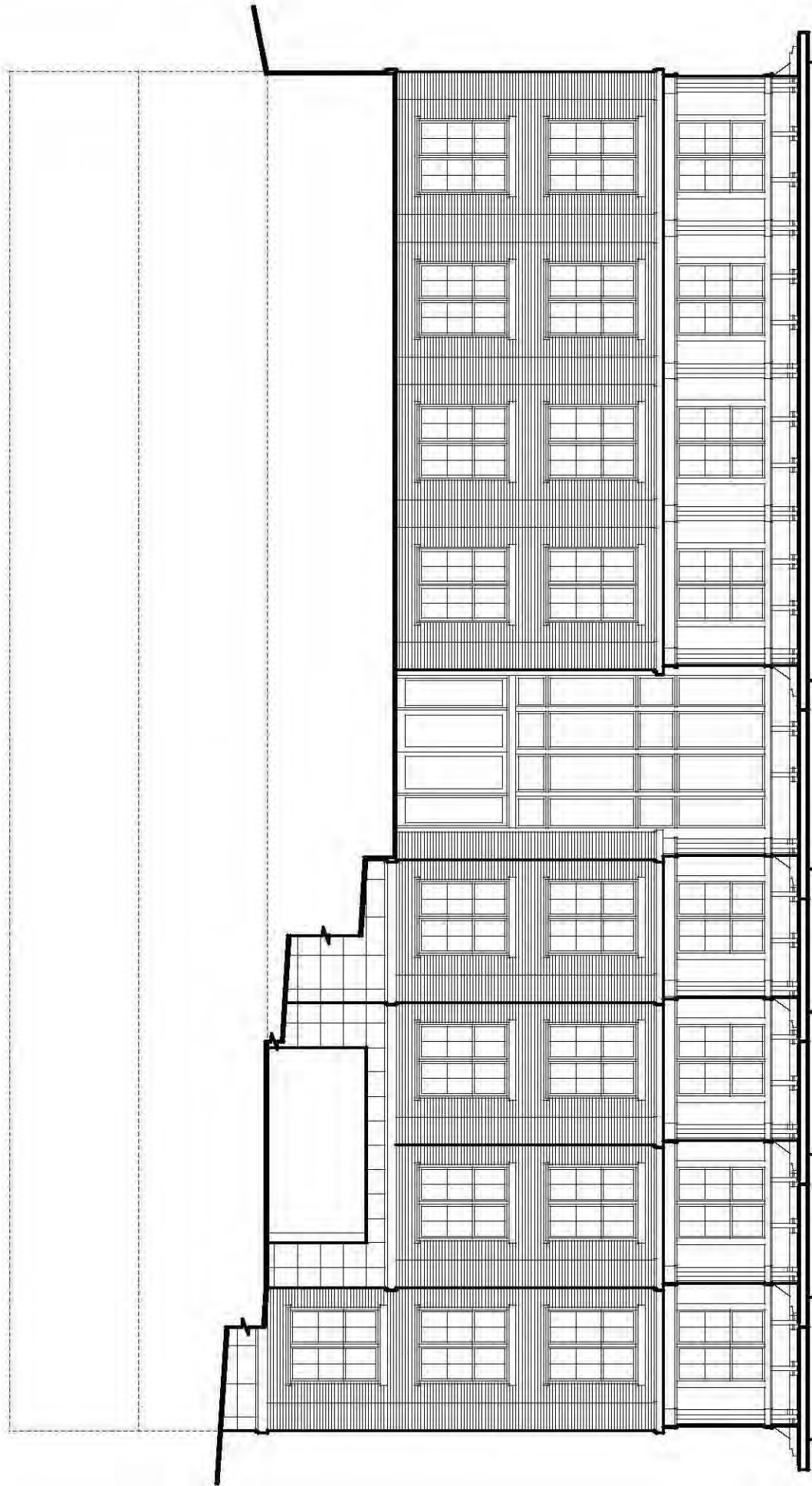
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1	4-21-15	SUP SUBMISSION
NO.	DATE	REVISION

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 INC.**

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 CHARLOTTESVILLE, VA 22902
 PHONE (434) 971-7202 FAX (434) 295-2413

1 NORTH (REAR) ELEVATION

3/32" = 1'-0"



SUP.09

1725 JEFFERSON PARK AVENUE
SPECIAL USE PERMIT
APPLICATION PACKAGE
BUILDING ELEVATION

JOB NUMBER 1409

FILE NO. 1409-SUP-WORKFILE

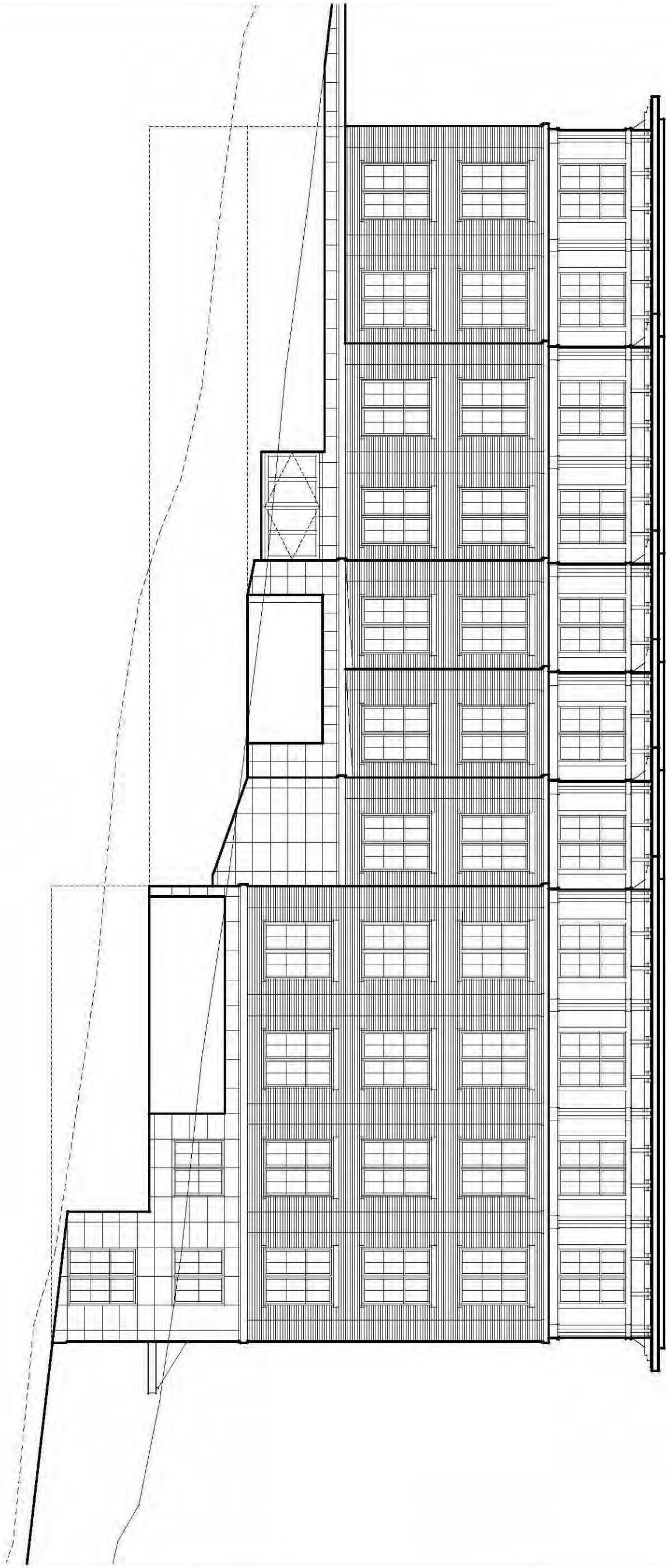
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1	4-21-15	SUP SUBMISSION
NO.	DATE	REVISION



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CHARLOTTESVILLE, VA 22902
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1 WEST (LEFT SIDE) ELEVATION



3/32"=1'-0"

SUP.10

1725 JEFFERSON PARK AVENUE
SPECIAL USE PERMIT
APPLICATION PACKAGE
BUILDING ELEVATION

JOB NUMBER 1409

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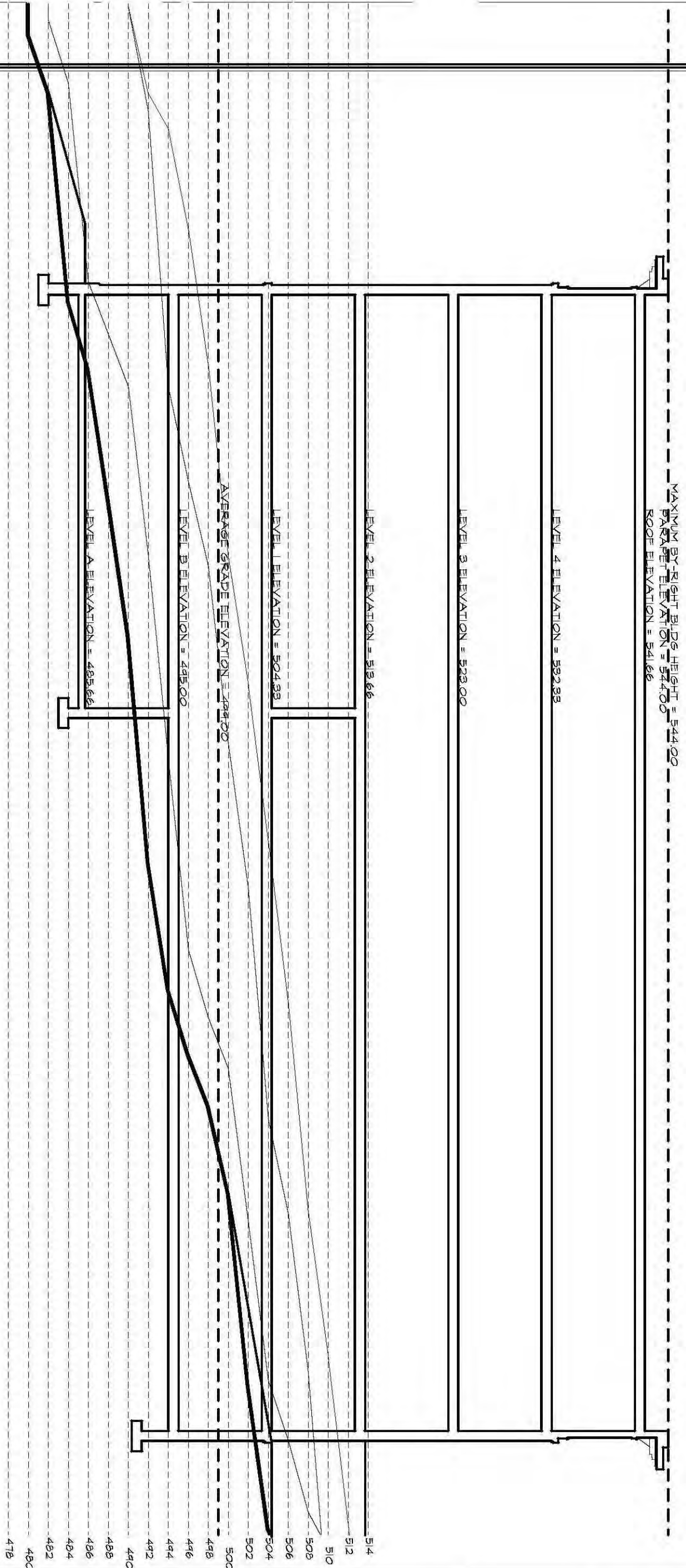
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ARCHITECTS
INC.**

1108 EAST HIGH STREET
CHARLOTTESVILLE, VA 22902
PHONE (434) 971-7202 FAX (434) 295-2413

SITE / BUILDING SECTION



3/32" = 1'-0"

514
512
510
508
506
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502
500
498
496
494
492
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484
482
480
478

SUP.11

1725 JEFFERSON PARK AVENUE
SPECIAL USE PERMIT
APPLICATION PACKAGE
SITE / BUILDING SECTION

JOB NUMBER 1409

FILE NO. 1409-SUP-WORKFILE

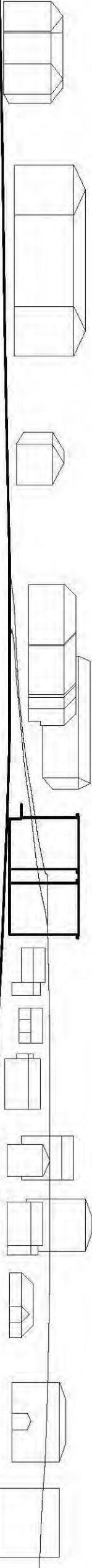
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1	4-21-15	SUP SUBMISSION
NO.	DATE	REVISION



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CHARLOTTESVILLE, VA 22902
PHONE (434) 971-7202 FAX (434) 295-2413

SITE & CONTEXT SECTION / ELEVATION (CUT THRU JPA)



3/32" = 1'-0"

SUP.13

1725 JEFFERSON PARK AVENUE
 SPECIAL USE PERMIT
 APPLICATION PACKAGE
 SITE & CONTEXT SECTION / ELEVATION

JOB NUMBER 1409 FILE NO. 1409-SUP-WORKFILE

NO.	DATE	DRAWING RELEASE
1	4-21-13	SUP SUBMISSION
NO.	DATE	REVISION

**ATWOOD HENNINGSEN KESTNER
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1725 JEFFERSON PARK AVENUE
CHARLOTTESVILLE, VIRGINIA
ADDITIONAL VIEW OF FRONT ENTRY


ATWOOD HENNINGSEN KESTNER
ARCHITECTS
INC.

REVISED DESIGN



1725 JEFFERSON PARK AVENUE
CHARLOTTESVILLE, VIRGINIA
SOUTH ELEVATION FROM ACROSS JPA



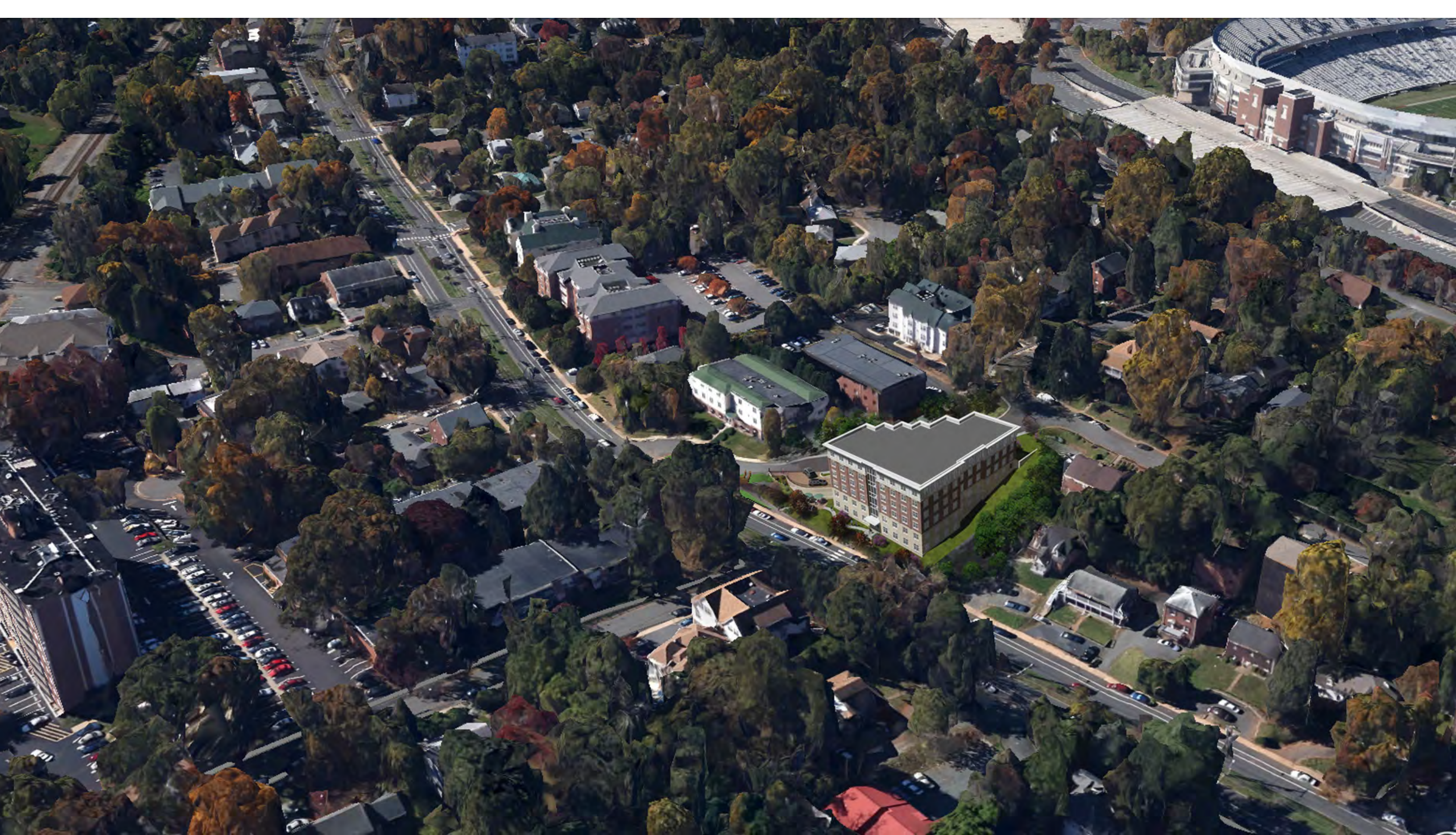
ATWOOD HENNINGSEN KESTNER
ARCHITECTS
INC.



1725 JEFFERSON PARK AVENUE
CHARLOTTESVILLE, VIRGINIA
PEDESTRIAN PERSPECTIVE FROM STEWART STREET



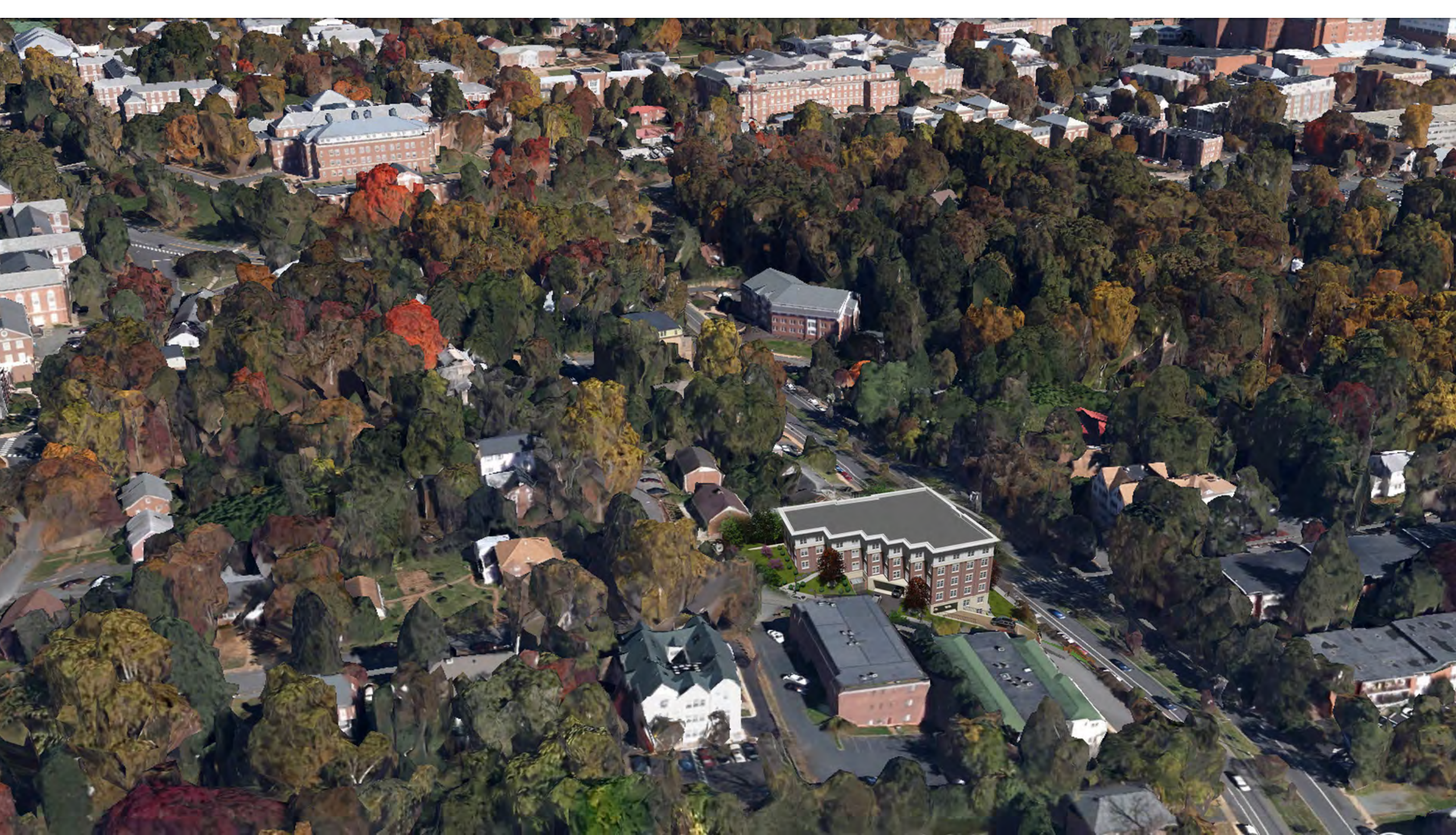
ATWOOD HENNINGSEN KESTNER
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1725 JEFFERSON PARK AVENUE
CHARLOTTESVILLE, VIRGINIA
BIRDS EYE VIEW FROM SOUTH EAST



ATWOOD HENNINGSEN KESTNER
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INC.



1725 JEFFERSON PARK AVENUE
CHARLOTTESVILLE, VIRGINIA
BIRDS EYE VIEW FROM NORTH WEST



ATWOOD HENNINGSEN KESTNER
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INC.



1725 JEFFERSON PARK AVENUE
CHARLOTTESVILLE, VIRGINIA
BIRDS EYE VIEW FROM SOUTH WEST



ATWOOD HENNINGSEN KESTNER
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INC.



CITY OF CHARLOTTESVILLE
DEPARTMENT OF NEIGHBORHOOD DEVELOPMENT SERVICES
STAFF REPORT TO THE ENTRANCE CORRIDOR REVIEW BOARD (ERB)

ENTRANCE CORRIDOR (EC)
CERTIFICATE OF APPROPRIATENESS

DATE OF PLANNING COMMISSION MEETING: August 11, 2015

Project Name: 1130 E High Street Mixed Use Development

Planner: Mary Joy Scala, AICP

Applicant: Richard Price, AIA

Applicant's Representative: Richard Price, AIA

Applicant's Relation to Owner: Same

Application Information

Property Street Address: 1130-1132 E High Street

Property Owner: Richard Price and Barbara Shenefield

Tax Map/Parcel: Tax Map 54, Parcels 52, 53 & 54 (Online Records: 540052000, 540053000, 540054000)

Total Square Footage/Acreage Site: 25,946 sq. ft./ 0.6 acres

Comprehensive Plan (Land Use Plan) Designation: Mixed Use

Current Zoning Classification: High Street Corridor with Entrance Corridor (EC) Overlay

Entrance Corridor Overlay District: §34-307(a)(10) East High Street/ 9th Street from Long Street to East Market Street

Current Usage: Two buildings (to be demolished) currently used for single family and office; and one vacant lot.

Background

The ERB reviews Entrance Corridor Certificate of Appropriateness applications when the proposal is for new construction.

Applicant's Request

The applicant is requesting approval of a certificate of appropriateness to construct four 2-3 story mixed-use buildings with 14 total units around a courtyard, with parking for maximum 15 cars.

The vehicular circulation includes one entrance from East High Street to access 5 surface parking spaces and 10 covered parking spaces tucked under buildings, including one HC space. There is an existing 5-foot sidewalk along East High Street. The main pedestrian access is a paved

forecourt that links the public sidewalk to the interior courtyard. A second pedestrian access connects the public sidewalk with the accessible parking space. Three existing street trees are proposed to be saved in the front yard, a 30" Maple, 30" Ash, and 18" Pecan. The front and rear yards will be planted with S1 buffers; a hedge will screen parking along the property line; the courtyard will contain a lawn and raingardens.

Building materials consist of painted cement fiber siding (hardiplank and hardipanel); and aluminum clad windows. The roofs are not visible behind parapets.

A monument sign is proposed at the main entrance.

Standard of Review

The Planning Commission serves as the entrance corridor review board (ERB) responsible for administering the design review process in entrance corridor overlay districts. This development project requires a site plan, and therefore also requires a certificate of appropriateness from the ERB, pursuant to the provisions of §34-309(a)(3) of the City's Zoning Ordinance. The ERB shall act on an application within 60 days of the submittal date, and shall either approve, approve with conditions, or deny the application. Appeal would be to City Council.

Standards for considering certificates of appropriateness:

In conducting review of an application, the ERB must consider certain features and factors in determining the appropriateness of proposed construction, alteration, etc. of buildings or structures located within an entrance corridor overlay district. Following is a list of the standards set forth within §34-310 of the City Code:

§34-310(1): Overall architectural design, form, and style of the subject building or structure, including, but not limited to: the height, mass and scale;

The proposed building complex is 2-3 stories in height, compactly arranged around a courtyard, with contemporary style elements. The roofs are essentially flat.

§34-310(2): Exterior architectural details and features of the subject building or structure;

The commercial space entrances face the forecourt and all residential units are entered from the shared courtyard. Fenestration is fixed, awning and casement windows, residential in scale.

The siding materials are varied in textures and colors.

No lighting has been shown, but will be full cutoff, including bollards in the courtyard, lighting in parking areas, and exterior fixtures on buildings. Signage is retrained; the monument sign will have illuminated text only.

§34-310(3): Texture, materials and color of materials proposed for use on the subject building or structure;

The proposed building materials consist of: painted cement fiber siding (hardiplank and hardipanel); and aluminum clad windows. The colors are not yet final, but are described as “warm earth tones” from Benjamin Moore “Historical Color” palette. The roofs are not visible behind parapets.

§34-310(4): Design and arrangement of buildings and structures on the subject site;

The building arrangement creates a nice, deep front yard that is compatible with the older development on the street. The courtyard is a strong design element that unifies the residential and commercial uses. The forecourt directs pedestrian activity toward the courtyard. Most of the parking is located under buildings to minimize visual impact. The landscape plan shows three large trees to be preserved along High Street, screening of parking from the adjacent property, and screening from the lower density zoning to the rear. There are rain gardens and a lawn area proposed in the courtyard to provide bioretention and privacy.

§34-310(5): The extent to which the features and characteristics described within paragraphs (1)-(4), above, are architecturally compatible (or incompatible) with similar features and characteristics of other buildings and structures having frontage on the same EC street(s) as the subject property.

The goals are to make the site function well for the users of this site and the corridor, and to have an attractive development that is compatible with its surrounding context.

The features and characteristics of the buildings and site described above will be architecturally compatible with other buildings on East High Street, even though it is a new, contemporary style development. Some of the features and characteristics that help make it compatible are: the mostly 2-story, but varied, building height; pedestrian scale; relegation of parking; orientation to the street; sustainable building materials; residential-scale windows; restrained signage and lighting; and residential-looking landscaping.

§34-310(6): Provisions of the Entrance Corridor Design Guidelines.

Relevant sections of the guidelines include:

Section 1 (Introduction)

The Entrance Corridor design principles are expanded below:

• Design For a Corridor Vision

New building design should be compatible (in massing, scale, materials, colors) with those structures that contribute to the overall character and quality of the corridor. Existing developments should be encouraged to make upgrades consistent with the corridor vision. Site designs should contain some common elements to provide continuity along the corridor. New development, including franchise

development, should complement the City's character and respect those qualities that distinguish the City's built environment.

- **Preserve History**

Preserve significant historic buildings as well as distinctive architecture from more recent periods. Encourage new contemporary design that integrates well with existing historic buildings to enhance the overall character and quality of the corridor.

- **Facilitate Pedestrian Access**

Encourage compact, walkable developments. Design pedestrian connections from sidewalk and car to buildings, between buildings, and between corridor properties and adjacent residential areas.

- **Maintain Human Scale in Buildings and Spaces**

Consider the building scale, especially height, mass, complexity of form, and architectural details, and the impact of spaces created, as it will be experienced by the people who will pass by, live, work, or shop there. The size, placement and number of doors, windows, portals and openings define human scale, as does the degree of ground-floor pedestrian access.

- **Preserve and Enhance Natural Character**

Daylight and improve streams, and retain mature trees and natural buffers. Work with topography to minimize grading and limit the introduction of impervious surfaces. Encourage plantings of diverse native species.

- **Create a Sense of Place**

In corridors where substantial pedestrian activity occurs or is encouraged, or where mixed use and multi-building projects are proposed, one goal will be creating a sense of place. Building arrangements, uses, natural features, and landscaping should contribute, where feasible, to create exterior space where people can interact.

- **Create an Inviting Public Realm**

Design inviting streetscapes and public spaces. Redevelopment of properties should enhance the existing streetscapes and create an engaging public realm.

- **Create Restrained Communications**

Private signage and advertising should be harmonious and in scale with building elements and landscaping features.

- **Screen Incompatible Uses and Appurtenances:**

Screen from adjacent properties and public view those uses and appurtenances whose visibility may be incompatible with the overall character and quality of the corridor, such as: parking lots, outdoor storage and loading areas, refuse areas, mechanical and communication equipment, Where feasible, relegate parking behind buildings. It is not the intent to require screening for utilitarian designs that are attractive, and/or purposeful.

- **Respect and Enhance Charlottesville's Character**

Charlottesville seeks new construction that reflects the unique character, history, and cultural diversity of this place. Architectural transplants from other locales, or shallow imitations of historic architectural styles, for example, are neither appropriate nor desirable. Incompatible aspects of franchise design or corporate signature buildings must be modified to fit the character of this community.

Section 2 (Streetscape)

Staff Analysis: The complex is correctly oriented to East High Street. The street trees and landscaping will create a nice frontage and a comfortable place to walk.

Section 3 (Site)

Staff Analysis: The courtyard site design unifies the uses on site, and the deep front, landscaped setback and pedestrian-oriented entrance make the new development feel compatible with the existing neighborhood.

Section 4 (Buildings)

Staff Analysis: Buildings of this height, mass and scale are appropriate in this area. Varied height, materials and color are used to break up the massing of the buildings, and respond to the diverse character of the neighborhood. Hardie siding and aluminum clad windows are recommended, sustainable materials.

Section 5 (Individual Corridors):

East High Street Vision

The southeast side of High Street from Long Street to the light at Meade Avenue shares similar characteristics with the Long Street corridor. Properties here have potential to be redeveloped at an urban scale with shallow setbacks, higher density, and mixed uses. The natural character of the river should be preserved, and riverfront properties may incorporate the river as a site amenity. Future infill and redevelopment on the northwest side of High Street from Riverdale Drive to Locust Avenue and on the southeast side of High Street from Meade Avenue to 10th Street should complement the smaller scale of the abutting residential neighborhoods on either side. The retail areas of this part of the corridor will continue to provide basic service-business functions until redeveloped into a mix of uses including residential. This area may be considered for nearby offsite or shared parking in the future, due to the small parcel sizes and convenience to transit and the downtown area. From Locust Avenue to Market Street there will be opportunities for denser development. The area surrounding Martha Jefferson Hospital is a potential historic district. A pedestrian environment should be encouraged along the entire corridor with sidewalks, landscaping and transit stops.

Recommended General Guidelines from Gillespie Street to 9th Street

- Respect the character of the older existing dwellings when designing infill buildings
- Place site parking behind buildings when converting residences to offices
- Maintain landscaped edge of private sites

Public Comments Received

No public comments have been received to date regarding the EC application.

Staff Recommendations

In staff opinion, the project as developed so far meets the standards and guidelines for a certificate of appropriateness in the Entrance Corridor. Staff recommends approval of this application subject to staff approval of final scaled drawings, provided the building design, materials, colors, site design, landscaping, lighting, and signage all remain essentially the same as described in the application packet dated July 15, 2015.

Suggested Motion

1. “I move to approve the Entrance Corridor Certificate of Appropriateness application for the new mixed use building at 1130 East High Street, with the following condition:

1. Staff approval of final scaled drawings, provided the building design, materials, colors, site design, landscaping, lighting, and signage all remain essentially the same as described in the application packet dated July 15, 2015.

2.....”

Attachments:

EC Application form (1 page)

EC Submission Packet dated 7/15/2015 (17 pages)



Entrance Corridor Review Application (EC)
Certificate of Appropriateness

RECEIVED

JUL 15 2015

Please Return To: City of Charlottesville
Department of Neighborhood Development Services
P.O. Box 911, City Hall
Charlottesville, Virginia 22902
Telephone (434) 970-3130 Fax (434) 970-3359

NEIGHBORHOOD DEVELOPMENT SERVICES

Please submit ten (10) copies of application form and all attachments.

For a new construction project, please include \$375 application fee. For all other projects requiring EC approval, please include \$125 application fee. For projects that require only administrative approval, please include \$100 administrative fee. Make checks payable to the City of Charlottesville.

The Entrance Corridor Review Board (ERB) meets the second Tuesday of the month.
Deadline for submittals is Tuesday 3 weeks prior to next ERB meeting by 3:30 p.m.

Owner Name Richard Price & Barbara Shenefield Applicant Name Richard Price AIA
Project Name/Description 1130 E High St Parcel Number TMP 54-52, 53 & 54
Street Address 1130-1132 E High St, Charlottesville VA 22902

Applicant Information

Address: 321 Parkway
Charlottesville VA 22902
Email: rwprice@ntelos.net
Phone: (W) 434.981.4239 (H) 434.923.3798
FAX:

Property Owner (if not applicant)

Address: same
Email:
Phone: (W) (H)
FAX:

Signature of Applicant

I hereby attest that the information I have provided is, to the best of my knowledge, correct. (Signature also denotes commitment to pay invoice for required mail notices.)

Signature [Handwritten Signature] Date 7/15/15
Print Name RICHARD PRICE Date

Property Owner Permission (if not applicant)

I have read this application and hereby give my consent to its submission.

Signature [Handwritten Signature] Date 7/15/15
Print Name RICHARD PRICE Date

Description of Proposed Work (attach separate narrative if necessary):

New, 14 unit mixed-use development, as described in detail in attached booklet.

Attachments (see reverse side for submittal requirements):

- (1) printed copy of booklet
(1) CD with electronic copy of booklet

For Office Use Only

Received by: Richard Price
Fee paid: \$375.00 Cash/Chk. # 6487
Date Received: 7/15/2015

Approved/Disapproved by:
Date:
Conditions of approval:

P15-0102



**New Development for 1130 E High St
Charlottesville VA**

**Application for Certificate of Appropriateness
High Street Entrance Corridor**

Submitted to: City of Charlottesville

Submitted by: Richard Price AIA

7/15/15

1. Overall Architectural Design

General Description
Project Team
Project summary
Location plan
Existing conditions
Proposed Massing

2. Exterior Details

Buildings 3D
Building elevations
Conceptual floor plans

3. Texture, Materials and Color

Material and color images

4. Design and Arrangement of Buildings

Current site plan
Proposed site plan

5. Compatibility with Other Buildings in EC

Description
Photographs

6. Compatibility with EC Design Guidelines

General Design Principles
Design Principles for High St Corridor
Site Design Principles
Building Design Principles

7. Landscape

Rendered site plan
Lighting

8. Signage

I. Overall Architectural Design

General Description

The proposed project is a mixed-use complex of fourteen (14) loft-like units arranged around a central courtyard. Two of the units, on the ground floor adjacent to High Street, will be used for commercial space. 10 of the units will be used for residential purposes, with live-work uses encouraged. The final two units, on the second story facing High Street, will be used for either commercial or residential purposes, depending on market demand. Four of the residential units will be ADA accessible.

The arrangement of units is intended to enliven the pedestrian experience on High St, while providing a shared, protected courtyard for residents. An entrance plaza, adjacent to the street, provides access to the commercial units, and to the residential courtyard. All residential units are accessed directly from the courtyard.

Taking advantage of existing topography, the majority of parking will be tucked under buildings to minimize visual impact.

Three significant trees will be preserved adjacent to High St, and the entire site re-planted with a palette of native trees and shrubs. The courtyard features a central lawn shared by all residents. Between the lawn and buildings will be intensely planted bioretention areas.

Project Team

The project will be designed, built and developed by a team led by PS2 Properties LLC, the design / development team behind the award-winning RiverBluff neighborhood in Charlottesville.

The team includes:

PS2 Properties LLC

Developer and General Contractor

Richard Price AIA

Site and building design

Kennon Williams Landscape Studio

Landscape design

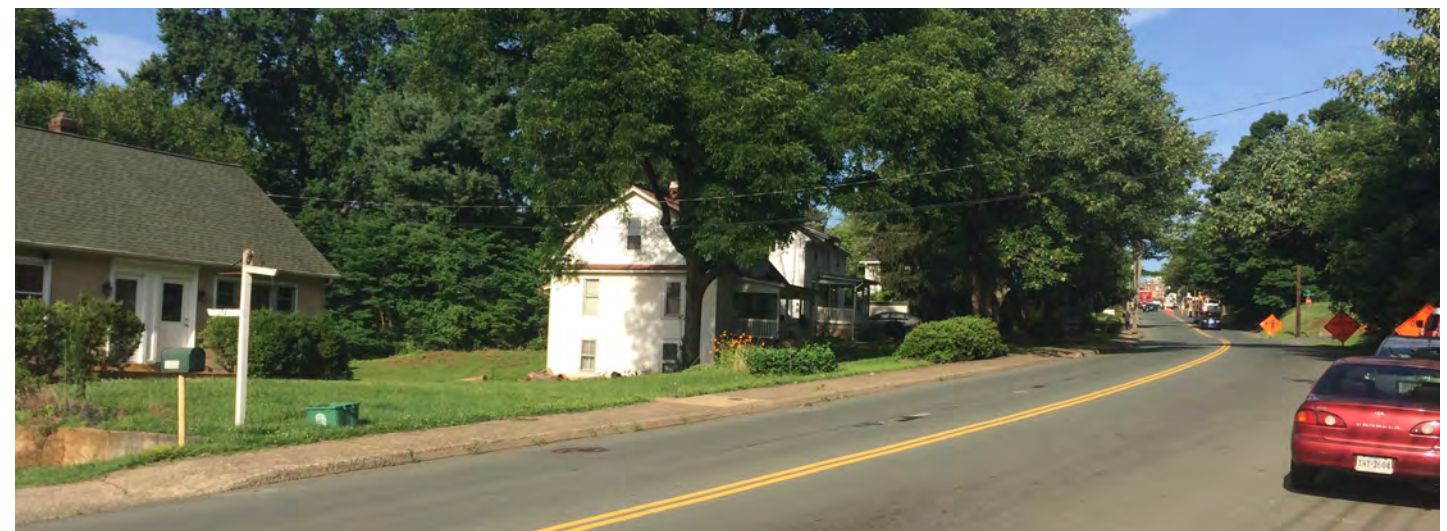
Project Summary

PROPOSED PROJECT	ALLOWED OR REQ'D	PROPOSED
Parcels:		
TMP 54-52, TMP 54-53, TMP 54-54	AREA 25,946	SF
	0.6	ACRES
ZONE	High St Mixed Use District	
	Entrance Corridor Overlay	
HEIGHT PER 34-67	35 ft	34 FT MAX
SETBACKS PER 34-678		
PRIMARY STREET (E HIGH) MIN FRONT YARD	15 FT	VARIES 17 - 22 FT
PRIMARY STREET (E HIGH) MAX FRONT YARD (MIN 50% OF FRONT YARD PLANTED WITH TYPE S1 LANDSCAPE BUFFER)	30 FT	
SIDE & REAR PLANTED BUFFER TYPE S1 [ADJACENT TO RESIDENTIAL]	10 FT	MIN 10 FT
USES		
RESIDENTIAL DENSITY	21 DU / ACRE = 12 UNITS	20 DU / ACRE MAX
1 OR 2 BR DWELLING UNIT PER 34-680		10 DU
OFFICE / 1 BR RESIDENTIAL UNITS [MAY BE USED FOR EITHER]		2 DU OR 1,450 GSF
OFFICE OR SIMILAR USE PER 34-676	NO LIMIT	1,950 GSF
PARKING PER 34-984		
1 OR 2 BR RESIDENTIAL UNITS - 1 / DU	10	
OFFICE / 1 BR RESIDENTIAL UNITS IF USED AS RESIDENTIAL - 1 per DU IF USED AS OFFICE - 1 per 500 GSF	2 OR 3	
OFFICE	4	
PARKING REDUCTION PER 34-985 (b) DISTANCE TO CTS BUS STOP < 600 FT	-2	
TOTAL	15 MAX	15

Location Plan



Existing conditions



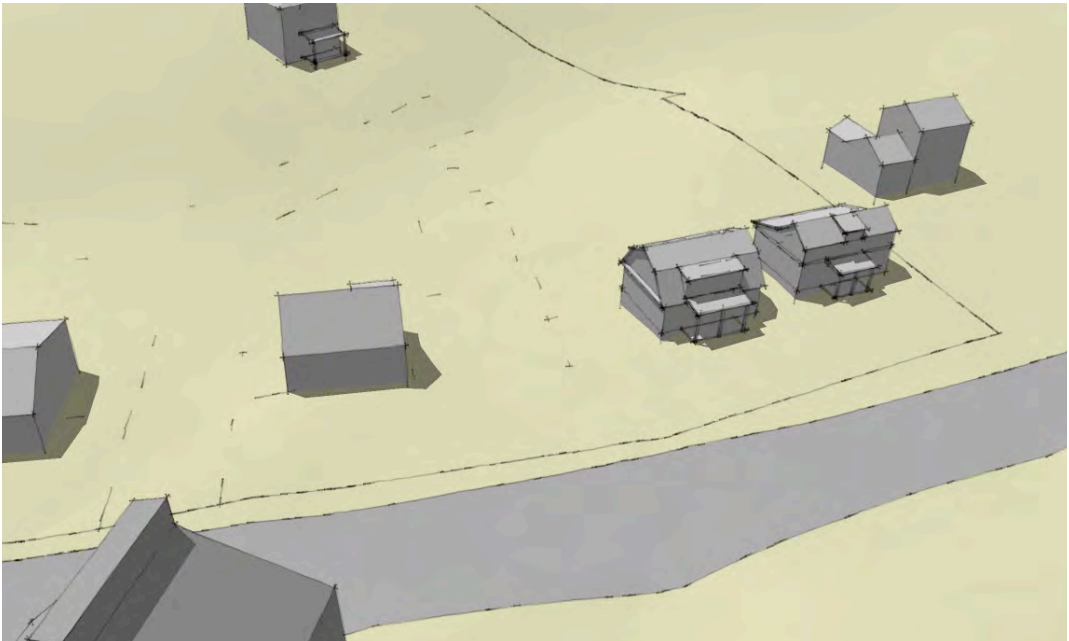
Building massing, scale and height

The immediate vicinity of the project consists of former residential buildings converted to commercial use. Buildings are generally set back from the street 30 - 50 ft.

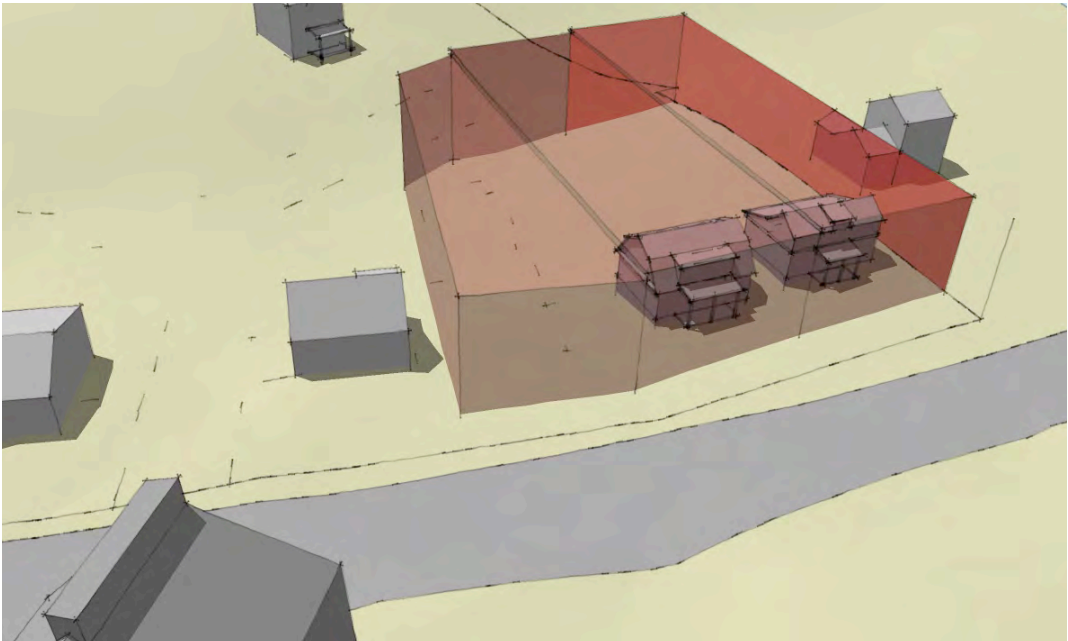
Current zoning allows a mixed-use project of up to 35 ft (3 stories) three stories, with no setbacks at sides. Front setbacks are required to be between 15 and 30 ft.

The project consists of two story buildings with a small third floor area on each. Some of the buildings will have a basement, which will take advantage of existing topography to accommodate storage and tuck-under parking.

Existing massing



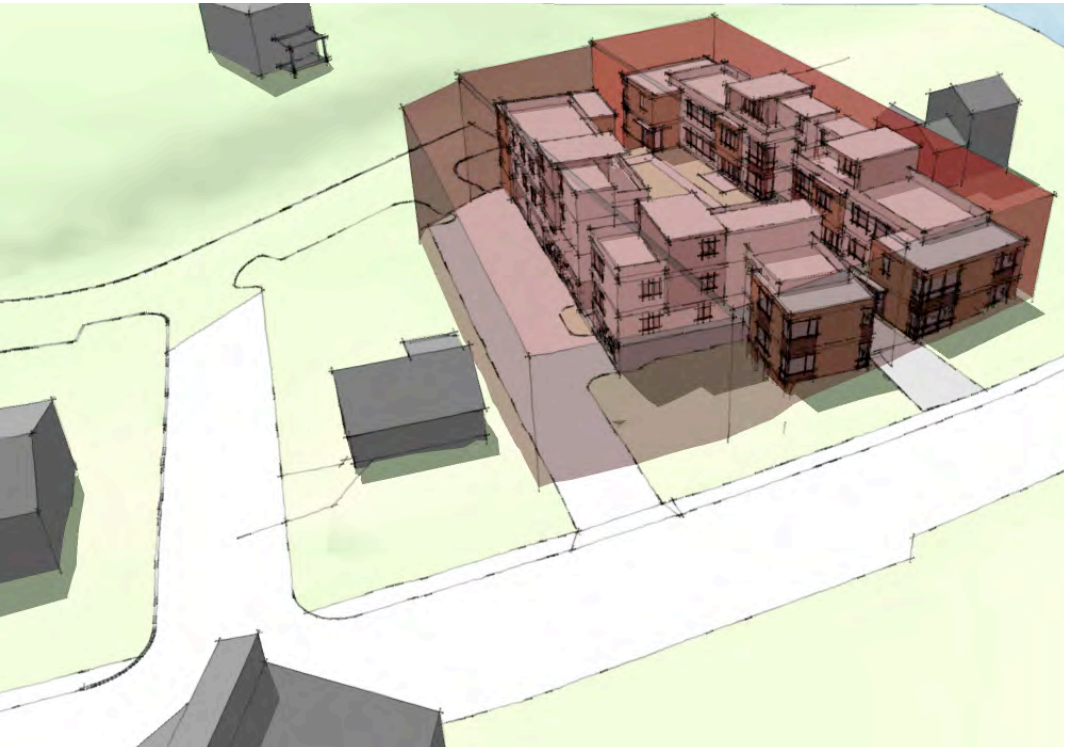
Allowable zoning envelope



Proposed massing



Proposed massing in relation to zoning envelope



Building Features

The project consists of 14 total units in four buildings. The buildings are arranged around a central courtyard, providing common space for all residents.

Two of the units, on the ground floor adjacent to High Street, will be used for commercial space. 10 of the units will be used for residential purposes, with live-work uses encouraged. The final two units, on the second story facing High Street, will be used for either commercial or residential purposes, depending on market demand. Four of the residential units will be ADA accessible.

Varied height, materials and colors will be used to break up the massing of the buildings, and respond to the diverse character of the neighborhood.





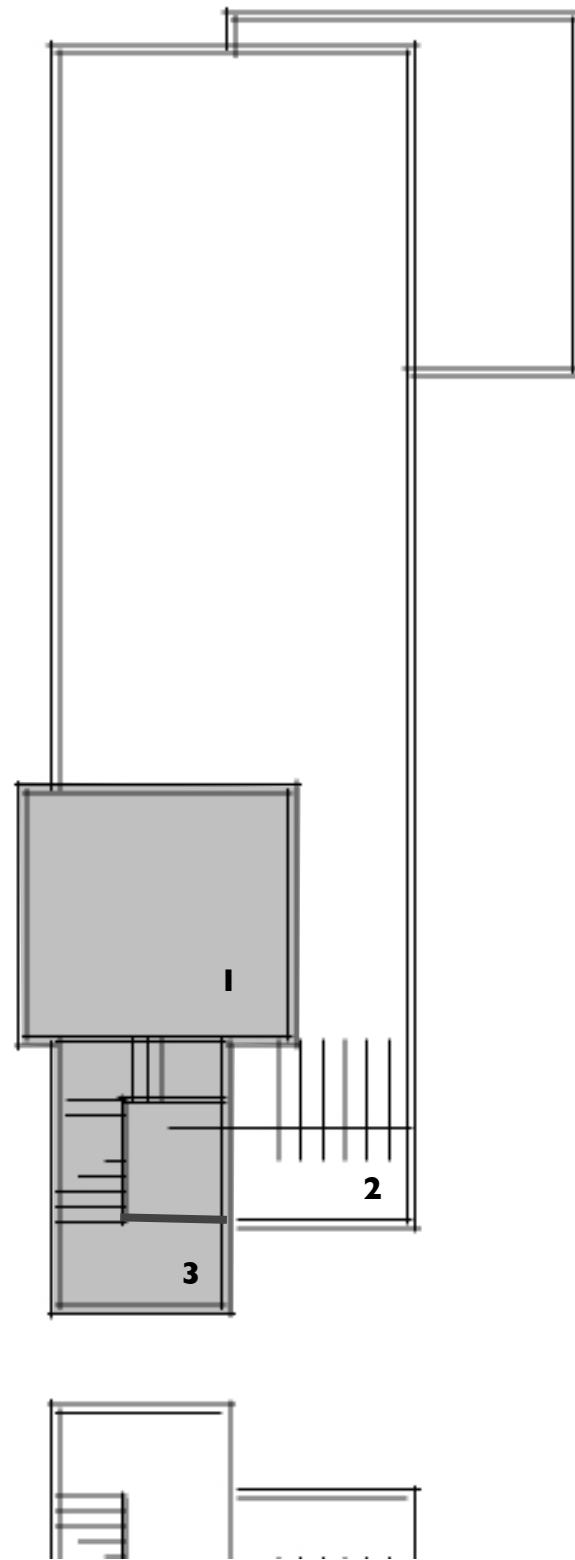
HIGH ST ELEVATION



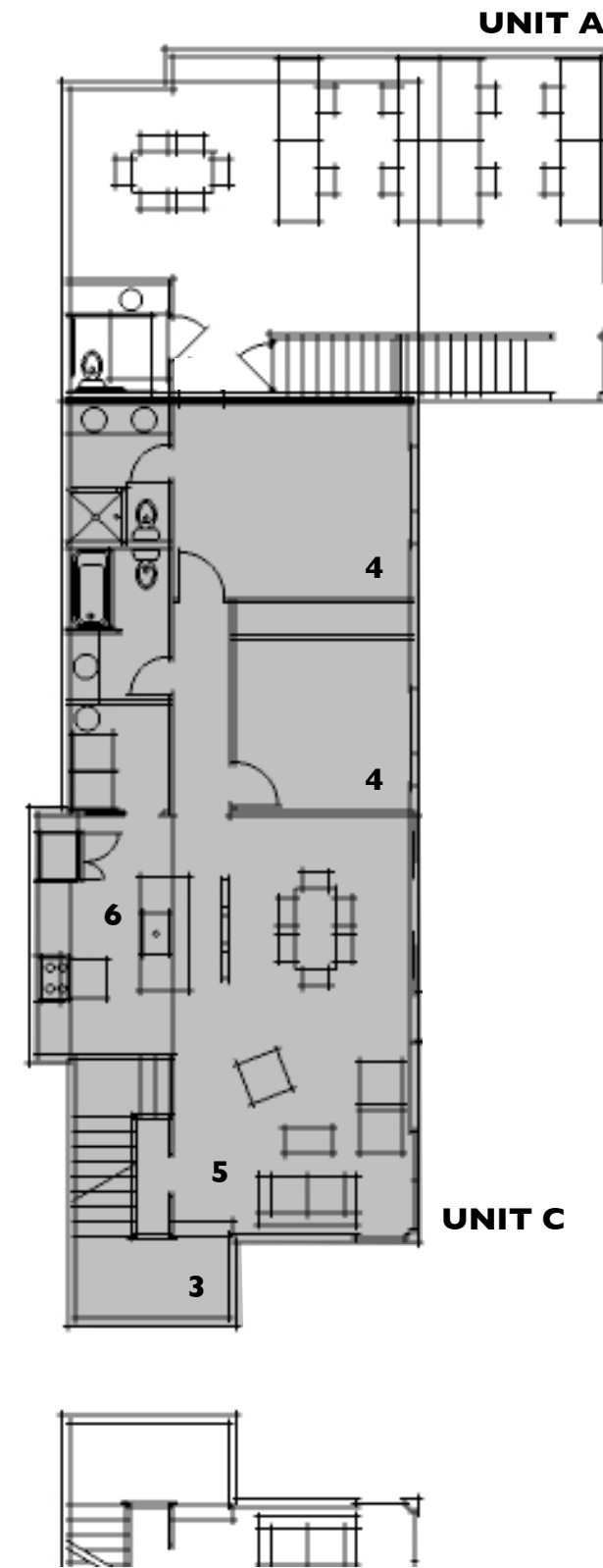
COURTYARD ELEVATION

**CONCEPTUAL FLOOR PLANS - BUILDING I
[PROVIDED FOR REFERENCE ONLY]**

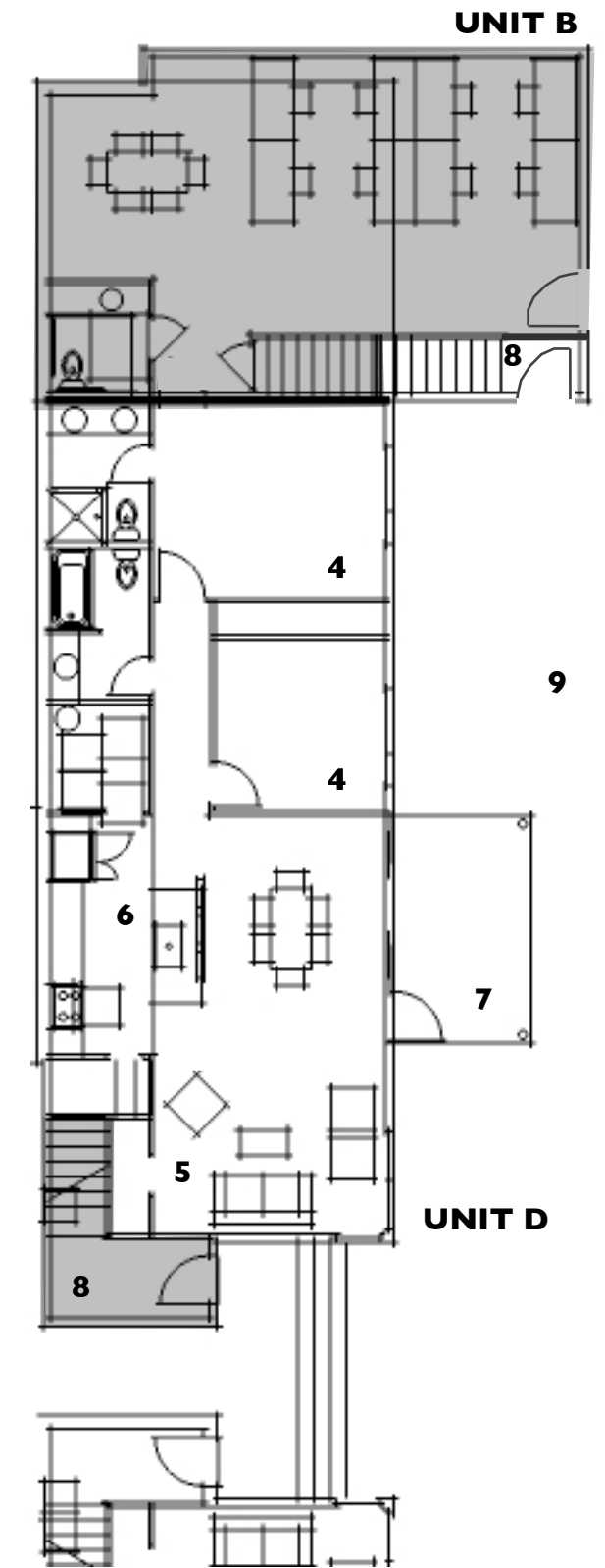
- 1. REC ROOM
- 2. ROOF TERRACE
- 3. STAIR
- 4. BR
- 5. LR / DA
- 6. KITCHEN
- 7. TERRACE
- 8. UPPER UNIT FOYER
- 9. COMMON SPACE



3



2



1

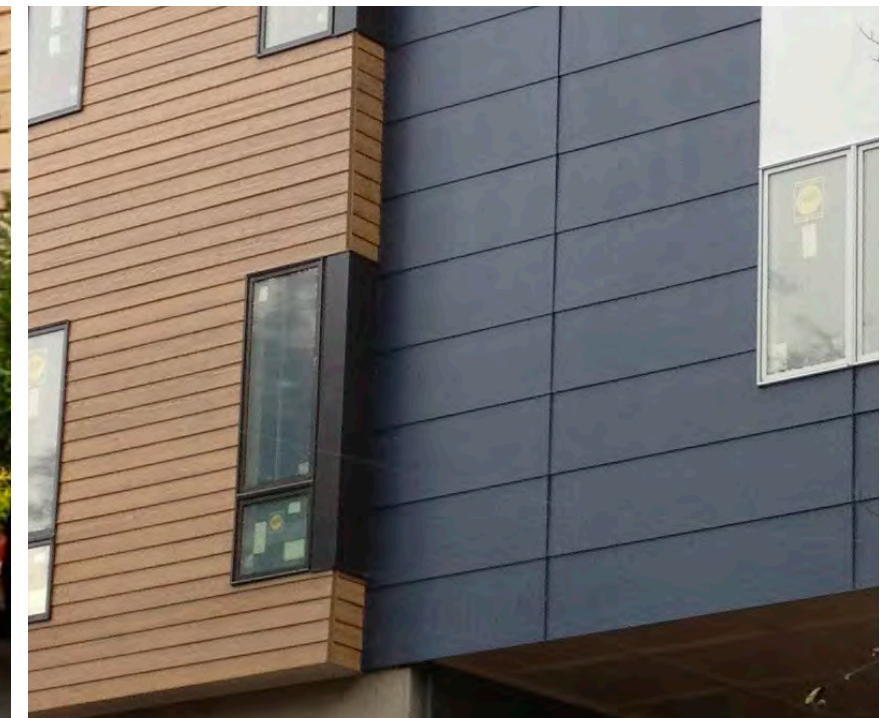
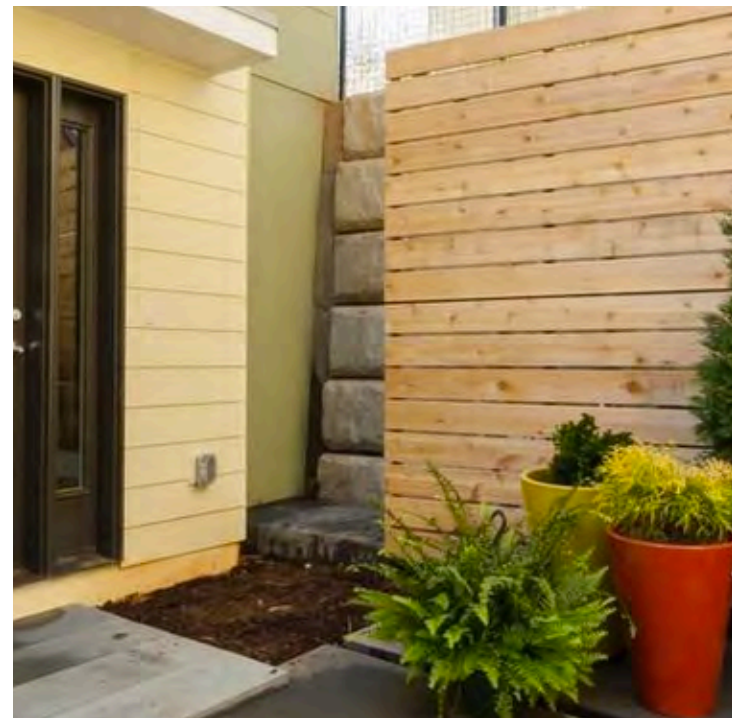
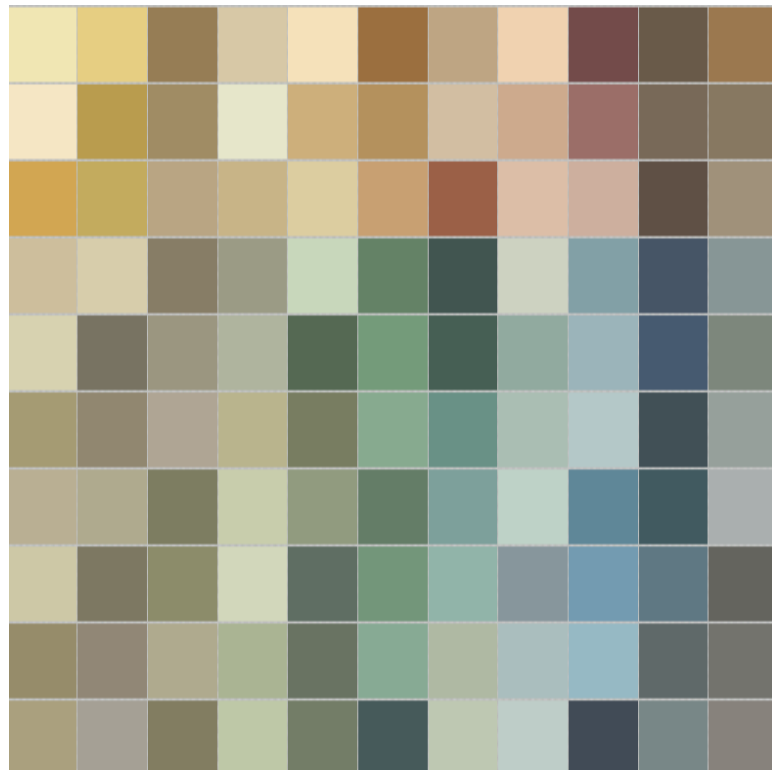
EXTERIOR MATERIALS

Materials, colors and details will be similar in character to details shown.

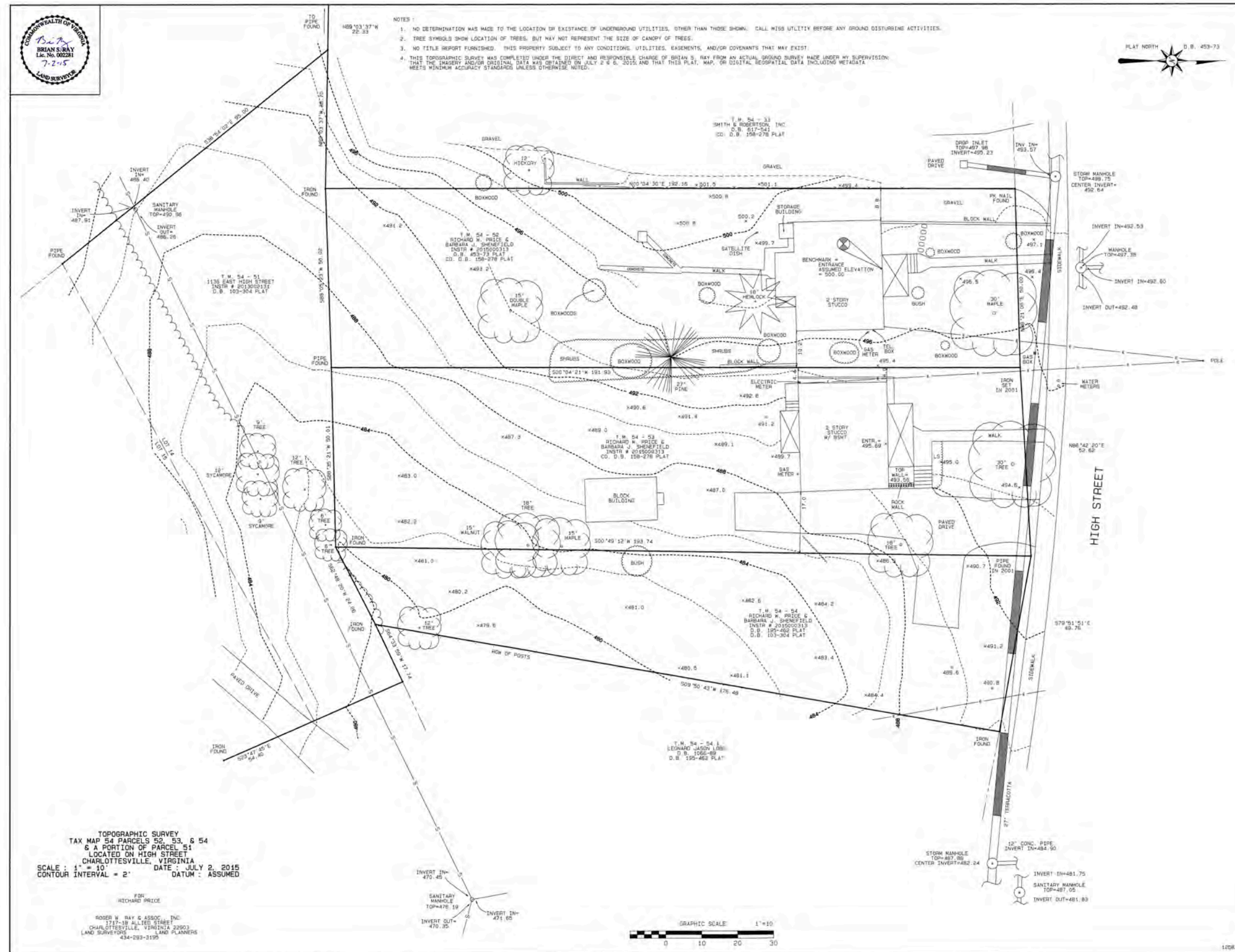
Siding: Painted cement fiber siding (Hardiplank and Hardipanel), in a variety of sizes and configurations, with limited use of natural wood accents.

Windows and doors: Aluminum-clad fixed, awning and casement windows. Flush and full-light doors.

Colors: Final selection will be made at a future date. Colors will be predominantly warm earth tones selected from Benjamin Moore standard “*Historical Color*” palette.

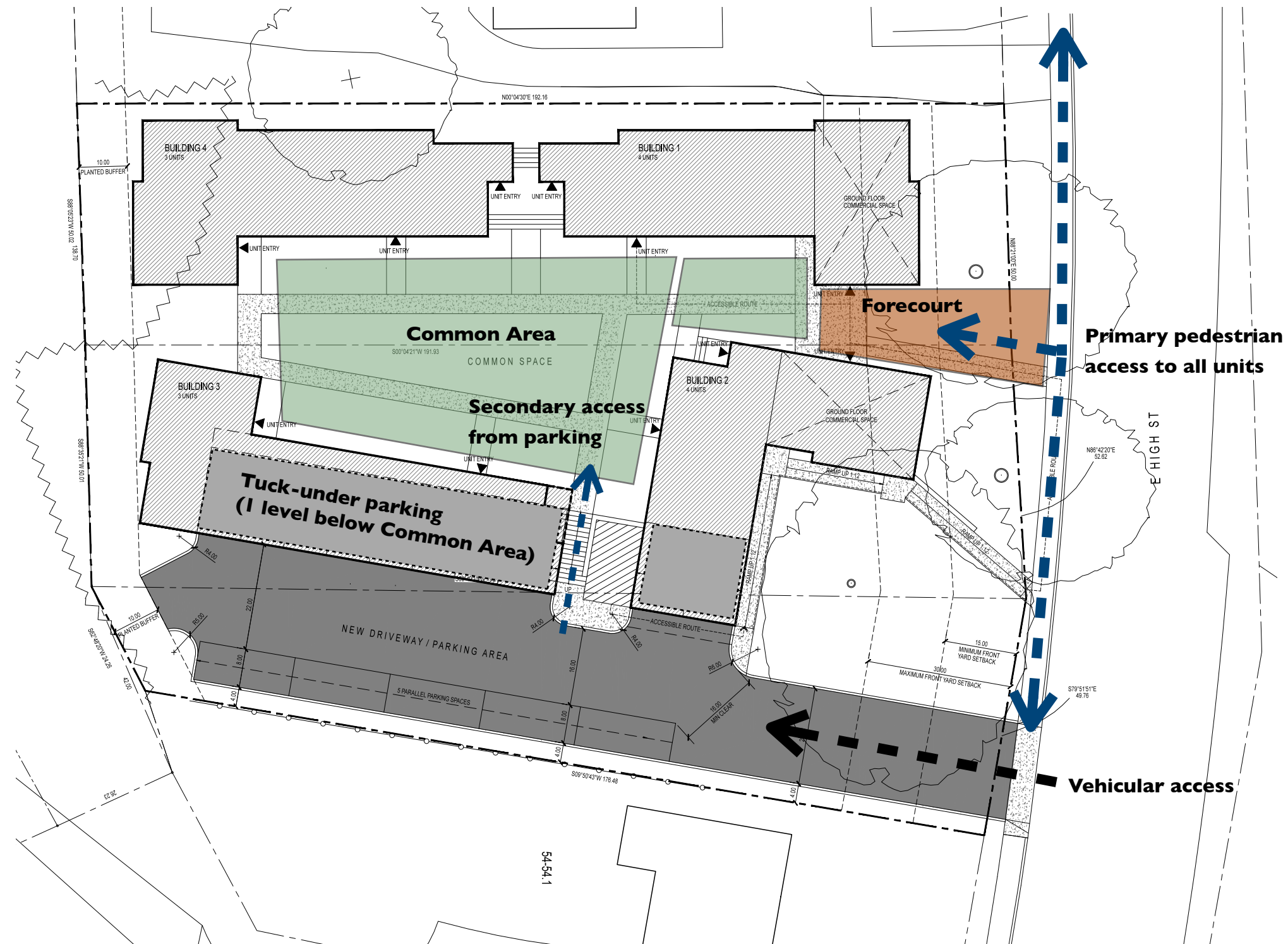


Existing site plan



4. Design and Arrangement of Buildings

Proposed site plan



5. Compatibility with Other Buildings in Entrance Corridor

COMPATIBILITY

The High St Entrance Corridor is a diverse mix of aging commercial structures and former residential units now used primarily for commercial purposes. There is no dominate style, size or roof configuration.

After many years of neglect, several buildings in the immediate vicinity have been recently renovated, and are beginning to define a more vibrant, eclectic character. These include the Fitch Services Building (2), Smith & Robertson (7), and the brightly painted commercial buildings to the rear of 1329 E High (3).

To respond to this diversity, the project has been designed with a varied massing, reflecting the spacing characteristic of the residential buildings and the footprints and roof form of the commercial structures.



6. Compatibility with Entrance Corridor Design Guidelines

GENERAL DESIGN PRINCIPLES

[Ref: ECDG Section 1C] The project has been designed to embrace the Entrance Corridor Design guidelines, and to comply with basic principles of high-quality urban infill development, as outlined below.

Design For a Corridor Vision

The buildings are designed to be compatible in massing, scale, materials and colors with the wide variety of buildings in the High St Corridor. [See page 12]

Preserve History

As is encouraged in the Design Guidelines, the project consists of new, contemporary buildings designed to be compatible with the massing of existing buildings in the corridor.

Facilitate Pedestrian Access

The project is designed to encourage a pedestrian-oriented lifestyle. A shared pedestrian corridor links the courtyard and the commercial units to the street. All residential units are entered directly from the courtyard.

Maintain Human Scale in Buildings and Spaces

The size and arrangement of buildings, windows and doors are residential in scale, and designed to respond to pedestrians rather than automobiles.

Preserve and Enhance Natural Character

The trees on site have been evaluated by a professional arborist. Three significant trees bordering High St have been pruned to improve health, and will be preserved. Other trees are deteriorated or nearing the end of their expected lifespan, and will be removed. The site will be replanted with native trees and shrubs. [See page 15]

Buildings have been designed to take advantage of the existing topography, and the majority of parking is located under buildings to minimize impervious surfaces.

Create a Sense of Place

The central courtyard is designed as a common space for all residents where the community can interact.

Create an Inviting Public Realm

Additional plantings will be added to enhance the pedestrian experience in the public right of way. [See page 15]

Create Restrained Communications

Exterior signage will be subtle and restrained. [See page 17]

Screen Incompatible Uses and Appurtenances

The parking area will be screened from the adjacent property with a planted buffer. [See page 15]

Garbage bins will be placed in storage areas adjacent to each covered parking space.

HVAC equipment will be located on rooftops behind parapet walls, and not be visible from the street level. Meters and connection boxes will be located to the sides and rear of buildings and screened with plants.

Community guidelines will require satellite dishes and other communications equipment to be shielded from view.

Respect and Enhance Charlottesville's Character

The project has been designed specifically for this site, to reflect a unique and interesting character compatible with the its context.

DESIGN PRINCIPLES FOR HIGH ST CORRIDOR

[Ref ECDG Section 4K] The project is designed to respond to the High St corridor's unique character.

Upgrade existing retail/service parcels with better defined parking, plantings and signs

The project is located at the transition from residential-scaled buildings to larger commercial buildings. It includes well-designed, screened parking, additional planting and well-designed signage.

Upgrade existing buildings as opportunities arise

The existing buildings are in poor condition and not well suited to mixed use. Current setback regulations would also require any new addition to be to the front of the buildings. The project hence includes demolition of the existing buildings and replacement with sympathetic new construction.

Consolidate parcels for larger new developments

The project consolidates 3 existing parcels of land.

Make new streetscape improvements to better define street edge

Building facade design, consistent setbacks and landscape design will work together to enliven the streetscape.

DESIGN PRINCIPLES- HIGH ST SUB AREA B

[Ref ECDG Section 4K] The project is designed to address to unique conditions in High St Sub Area B.

Respect the character of older existing dwellings

The buildings have been designed to reflect the scale and massing of existing buildings in the vicinity.

Place site parking behind buildings

Parking is located to the rear and tucked under the proposed buildings.

Maintain landscaped edge of private sites

The edge property will be planted to create a buffer from adjacent sites. The adjacent properties each have a fence along the property lines that will be protected during construction. [See page 15]

6. Compatibility with Entrance Corridor Design Guidelines

SITE DESIGN PRINCIPLES

[Ref: ECDG Section 3] The project has been designed and configured to be an integral part of the surrounding neighborhood and ecosystems.

B. Connectivity between Entrance Corridor Areas & Neighborhoods and

C. Connectivity between & within Sites

The project is designed to encourage a pedestrian-oriented lifestyle. A shared pedestrian corridor links the courtyard and the commercial units to the street. A secondary access connects the parking areas to the courtyard. All residential units are entered directly from the courtyard. [See page 11]

D. Building Placement

Buildings are designed to comply with guidelines, including fronting on the High St corridor, and a compact and contiguous building arrangement.

E. Parking

Parking is designed to minimize visual impacts thru the use of tuck under parking, and screening the parking area from adjacent properties.

F. Plantings & Open Spaces

The landscape is designed to complement the building design and design standards. It will consist primarily of natives and native cultivars, including shade trees, buffers and ornamentals as appropriate. Stormwater will be managed in the courtyard using planted bioretention areas.

[See page 15]

G. Lighting

All exterior lighting will be full cutoff, including bollards in courtyard, lighting in parking areas, and exterior fixtures on buildings. The building signage will have illuminated letters only. [See page 16]

H. Walls & Fences

No new walls or fences are currently planned. Fences on adjacent property will be retained at their owner's discretion.

I. Signs

Signage is designed to be subtle and to complement the design of the project. Signage will comply with the design guidelines and the requirements of the Zoning Ordinance. [See page 17]

J. Utilities, Communication Equipment & Service Areas

HVAC equipment will be located on rooftops behind parapet walls, and not be visible from the street or adjacent properties. Meters and connection boxes will be located to the sides and rear of buildings and screened with plants.

Community guidelines will require satellite dishes and other communications equipment to be shielded from view.

BUILDING DESIGN PRINCIPLES

[Ref: ECDG Section 4] The project has been designed to embrace the building design principles of the Entrance Corridor Design Guidelines.

B. Architectural Compatibility

The project is designed for this unique site, and responds to the diverse character of the corridor, including former residences converted to commercial use and larger scale commercial buildings.

C. Building Mass, Scale & Height

The buildings include varied massing, generally two stories in height with some third story space. Building massing fronting High St is designed to reflect the spacing and scale of converted residential buildings in the vicinity.

D. Facade Organization & Storefronts

Commercial space entrances face the forecourt to minimize impact on existing trees and help enliven the forecourt. All residential units are entered from the shared courtyard. Fenestration is residential in scale, using a pattern that emphasizes the entry and forecourt as a primary outdoor space.

E. Materials & Textures and

F. Color and

G. Details

Materials and details have been chosen to provide a variety of texture and colors compatible with the mixed residential and commercial character of the Corridor. [See page 12]

H. Roof Form & Materials

Consistent with nearby commercial buildings, roofs are low-slope and hidden behind parapet walls. Roofs will not be visible from the street.

I. Awnings

No awnings are planned.

J. Appurtenances

There are no appurtenances on the project.

Sections K, L, M, N

Not applicable to this project.

O. Multi-family Buildings

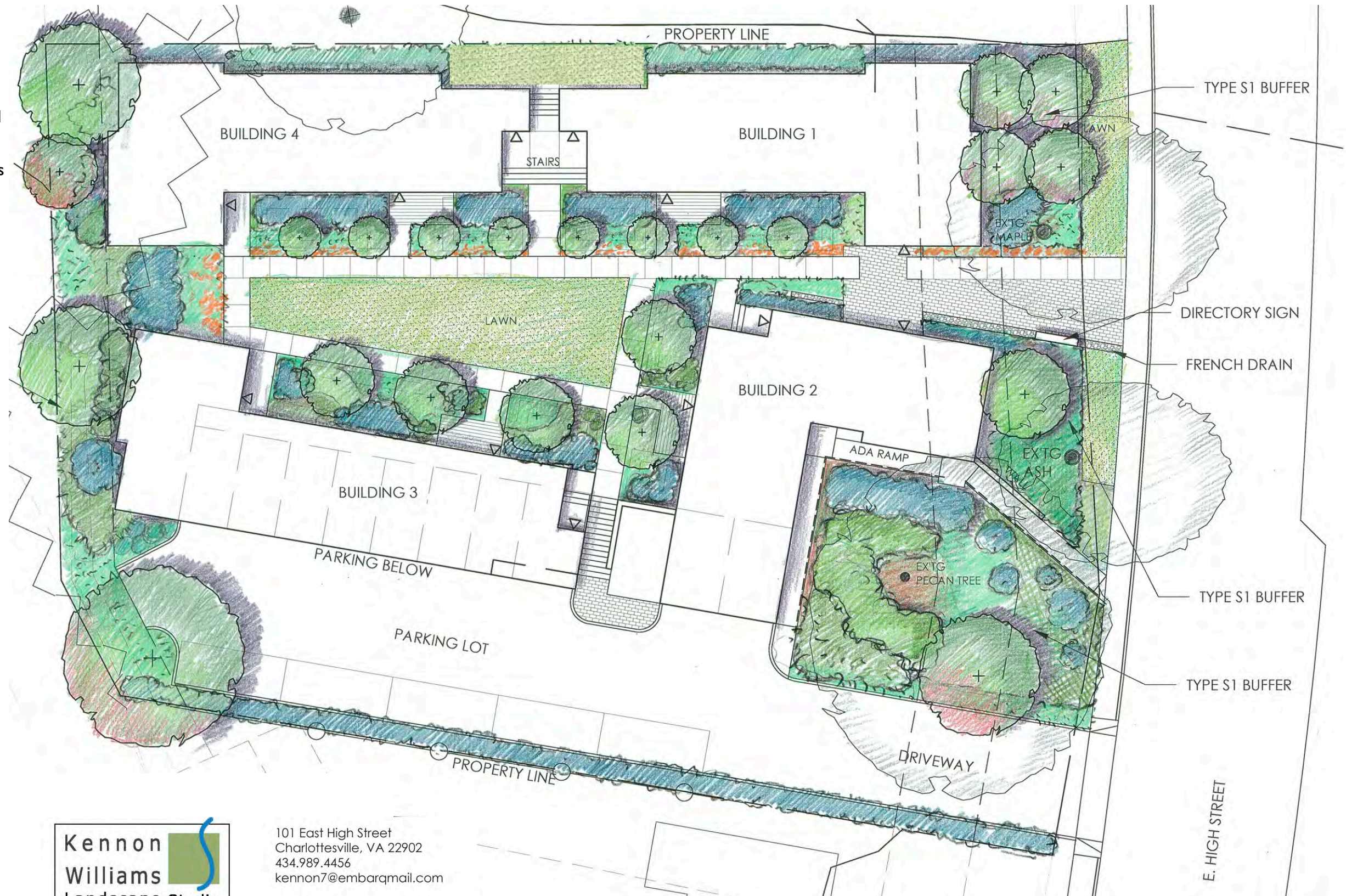
The residential portions of the project have been designed to integrate with the commercial areas. Live / work and home-based businesses, as allowed by applicable ordinances, will be encouraged.

Landscape Design

A paved forecourt will create a shared project entry. The residential units will front on to a shared courtyard, focused on a lawn at the center. The areas between the lawn and the building faces will be used as raingardens, and will be planted to provide bioretention and privacy.

Three significant trees will be retained in the front. The front and rear yards will be planted with a Type S1 buffer as required by regulations.

A hedge will be planted to the side of the parking area to buffer the parking from adjacent properties.



101 East High Street
 Charlottesville, VA 22902
 434.989.4456
 kennon7@embarqmail.com

LIGHTING

All exterior lighting will be full-cutoff, low brightness fixtures, selected to comply with dark sky standards.

All lighting will be bollard lights or building mounted. No flood lights or post lights will be used in the project.

The fixtures shown are fixtures used successfully on other projects. Fixtures for this project will be similar in performance and character.



Recessed exterior soffit lights

Line voltage, full cut off, max 10 foot-candles.



Step lights

Line voltage, full cut off, max 5 foot-candles.



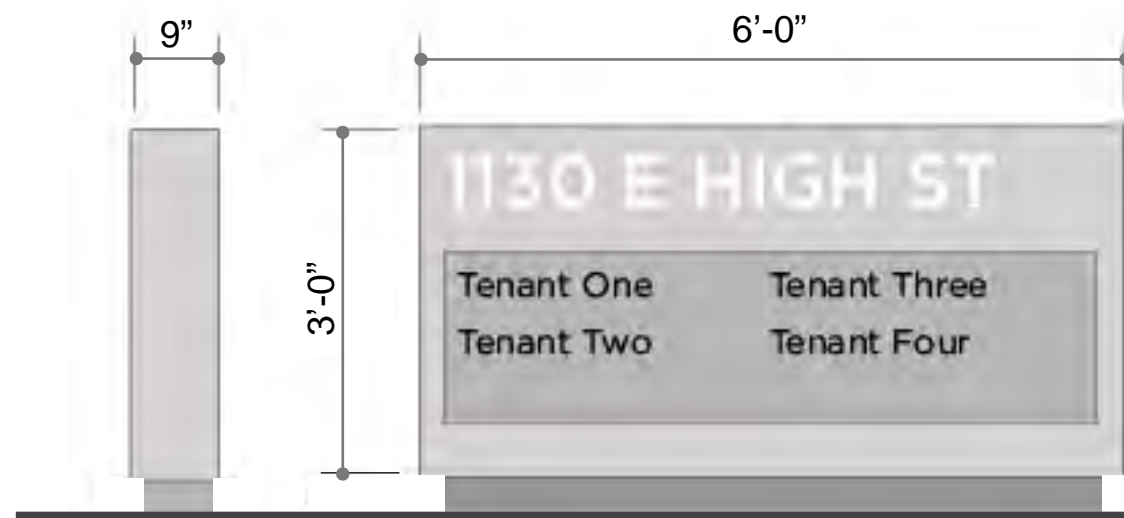
Bollard light

Line voltage, full cut off, max 5 foot-candles.



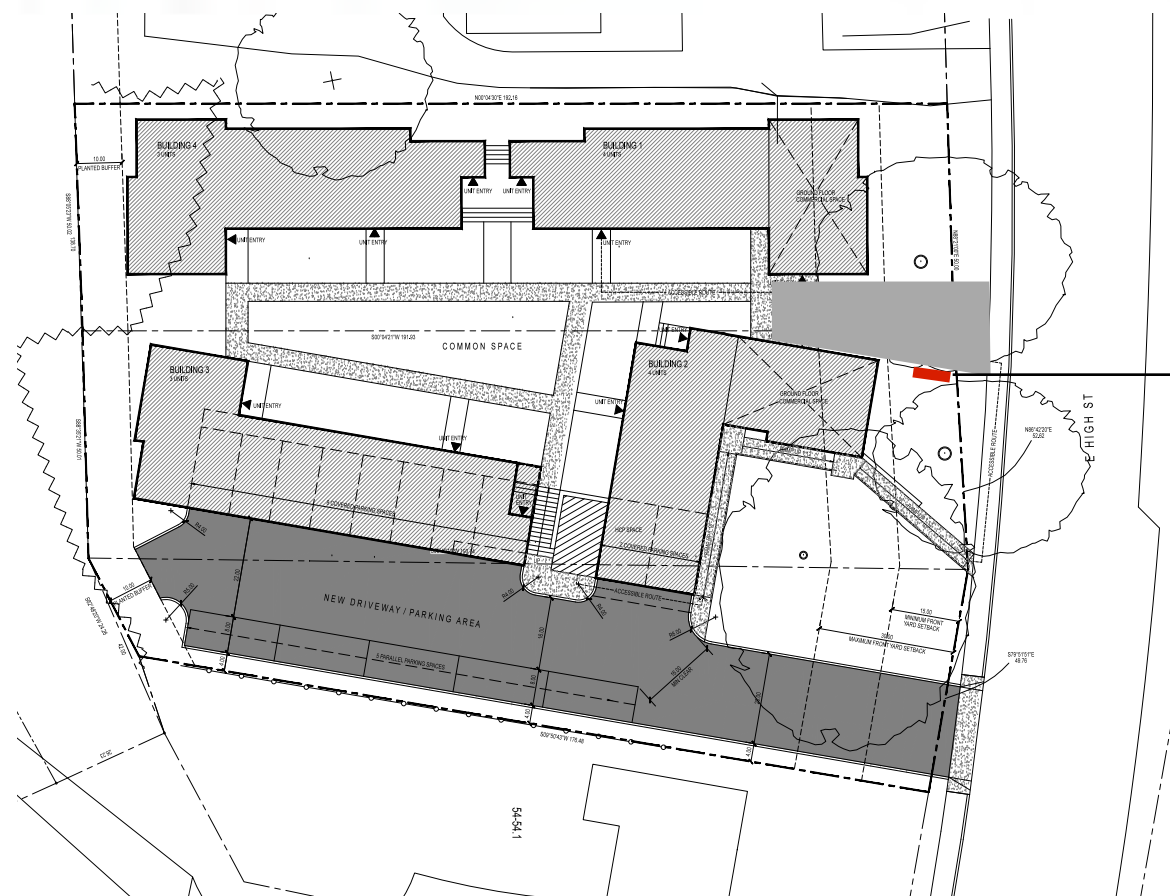
Unit numbers

Pin-mounted 5" cast aluminum numbers.



Monument-type street sign

Ground mounted, painted metal, internal illumination of letters and numbers only.



Sign location

CITY OF CHARLOTTESVILLE, VIRGINIA PLANNING COMMISSION DISCUSSION



Agenda Date: August 11, 2015

Staff Contacts: Missy Creasy, Interim Director, NDS

Carrie Rainey, Urban Designer, NDS

Subject: West Main Street Zoning Code Draft Considerations

Background

West Main Street is a dynamic corridor that is experiencing an influx of new development and redevelopment/revitalization of existing structures. Over the past few years, there have been a number of development projects both proposed and constructed along West Main Street, particularly west of the Bridge. Many of these developments have been designed to maximize height and bulk. Of the developments constructed along the corridor, many have been perceived by the public as too large, too tall, lacking in open spaces and character, and not compatible with adjacent streets and neighborhoods.

West Main Street is comprised of an eclectic mix of buildings, where the pattern of development occurring east of the bridge is of smaller scale than the pattern of development on the west side. West of the bridge, newer buildings, such as the University of Virginia Children's Hospital and The Flats residential building, are taller and larger in scale compared to their historic and contributing neighbors. East of the bridge, more historic and contributing buildings, comprised of 1-2 story businesses and restaurants, have survived, creating a lower skyline. Buildings provide an important "structure" to the public realm of the street. The built edge along West Main Street is uneven, with gaps and openings along the entire corridor. These gaps are typically comprised of driveways and parking lots. Buildings located close to the street create a rhythm of storefronts, porches, and outdoor cafes, all of which activate the street. Buildings such as the First Baptist Church and Amtrak Train Station are notable buildings on West Main and are important landmarks. Many older structures are set back from the street and a number of large parcels along West Main Street are undeveloped or paved as parking lots, creating a number of potential future development sites. The topography of the street also contributes to the diversity of the street.

Zoning is a tool often used by communities to help guide and manage development. The proposed code changes seek to alleviate the concerns revolved around development in the West Main corridor by establishing clear building envelopes, reducing allowable heights, and discouraging monolithic facades.

West Main Street is an Architectural Design Control District (ADC) due to its unique architectural and historic value. All properties are subject to review by the Board of Architectural Review (BAR) for any exterior construction, reconstruction, alteration, or restoration (see Section 34-275- *Certificates of appropriateness; construction and alterations* of the City Code of Ordinances for more information). In addition, no contributing structure may be demolished without BAR approval (see West Main Street Zoning Map). The ADC Guidelines, last amended on December 2, 2013, assist applicants with creating appropriate designs for projects in the corridor. The BAR utilizes the guidelines and discretion to determine if proposed projects are appropriate in context and detail. Please see the Council reports for additional information, found at: <http://www.charlottesville.org/index.aspx?page=3661> under the documents for the May 18th 2015 Meeting.

The proposed code changes were originally intended to be discussed at the July 28th 2015 Planning Commission work session. Time expired on the work session before the proposed changes were discussed, but public comments were received from representatives from the University of Virginia and Southern Environmental Law Center, and advocates for trees and bicycle facilities.

Summary of Proposed Changes

Staff recommends incorporating several key components from the proposed form based code (FBC) for West Main Street, as well several additional changes, into the existing zoning ordinance to continue moving the community towards its goals (bolded text reflects additions requested as a result of the May 18, 2015 Council meeting):

1. Reorientation of zoning to be categorized by east-west instead of north-south differentiations and associated modifications to uses categories **and building setbacks**. (see *Article 2.2: Districts, Article 3: Land Use*)
2. Reduced building height of 75-feet west of the bridge, and 52-feet east of the bridge with no additional heights allowed through Special Use Permit. **Require a setback of ten (10) feet after forty (40) feet in height for both districts**. (see *Article 2.2: Districts*)
3. Bulk plane requirements to step down large buildings to the same scale as adjacent residential districts along shared property lines. (see *Article 2.1.K.1: Rules Applicable to All Districts, Neighborhood Compatibility, Bulk Plane*)
4. **Retain a by-right residential density of forty-three (43) DUA (dwelling units per acre). Allow up to two hundred (200) DUA by special use permit (SUP) in both WM-1 (renamed *West Main Street West* in proposed code sections) and WM-2 (renamed *West Main Street East* in proposed code sections). Please note the existing zoning ordinance allows up to two hundred (200) DUA by SUP in WMN and two hundred forty (240) DUA in WMS.**
5. No parking required for new or existing retail under 5,000 square feet in floor area. (see *Article 4.2.A.2: General Development Standards, Off-Street Parking and Loading*)

6. New bike parking regulations for short- and long-term parking based on enclosed floor area. *(see Article 4.3.A-B: General Development Standards, Bicycle Parking)*
7. **Move the parcels collectively known as the Amtrak site (808-840 West Main Street) to the proposed zoning category WM-2 (*West Main Street East*).**
8. **Require a building break for every two hundred (200) feet of building length. It is recommended that staff and Planning Commission further study what shall be considered an adequate building break.**

It is important to note that these recommended additions will limit SUPs and the related Council and Planning Commission review to density and certain uses, and will remove SUP review for height. Review by the BAR will remain as it is today.

Points for Discussion

During the drafting of the proposed code changes, staff encountered several decision points which the Planning Commission may wish to discuss. These issues arise in the variation between proposed sections of code drafted from the proposed form based code (FBC) document created by Code Studio and the existing code. In the draft code sections, staff has proposed language that strives to meld the proposed code changes with the existing code. These decisions and their considerations are discussed below:

1. Relabeling of districts: The proposed FBC suggests labeling the western portion of West Main Street as WM-1 and the eastern portion as WM-2. In the draft code sections, staff has amended these labels to be **West Main Street West** and **West Main Street East** to better fit the existing system of mixed use corridor labeling and to reduce confusion as to which area of the street a particular code section applies.

Which system of labeling the districts does the Planning Commission prefer?

2. Designation of primary and linking streets: The proposed FBC suggests a new way to designate primary and linking streets. A list of primary streets is given, with all other streets falling into the linking category. This format is used in the proposed code draft. A second option is to continue with the designation format in the existing code. Potential traditional designations:

a. West Main Street West

- I. *Primary streets:* Jefferson Park Avenue, Wertland Street, 10th Street NW, Roosevelt Brown Boulevard, and West Main Street.
- II. *Linking streets:* 12th Street NW, 11th Street SW, and 9th Street SW.

b. West Main Street East

- I. *Primary streets:* 7th Street SW, 4th Street NW, Ridge Street, South Street, Commerce Street, and West Main Street.

- II. *Linking streets:* 8th Street NW, Cream Street, 7th Street NW, 6th Street NW, and 5th Street SW.

Which system of labeling primary and linking streets does the Planning Commission prefer?

3. The proposed FBC designated heights for building stories that do not correspond with the existing code designations. Staff has designated minimum and maximum heights by story in the proposed code draft, which will follow heights specified in *Section 34-1100 Height- Application of district regulations*. Following this code section, a maximum of six (6) stories in height is equal to seventy (70) feet. In contrast, the proposed FBC specified a height of seventy-five (75) feet for a six story building. Alternatively, the Planning Commission may consider amending the West Main Street districts to have alternative height allowances as specified in the FBC. While considering these options, Planning Commission may wish to consider item 4 below in conjunction, as required minimum ground floor heights may affect achievable story counts.

Which option does the Planning Commission prefer?

4. Per request of Council, staff has added a section of code (proposed Sec. 34-618(c) and 34-838(c)) limiting allowable building width before a differentiation is required. Staff has provided language that is not detailed to allow the Board of Architectural Review (BAR) flexibility in determining what constitutes an adequate break based on building context. Additional options include requiring an inset at a maximum spacing, or requiring different materials at a maximum spacing. However, these options may not achieve the desired results and limit the BAR's ability to require changes from applicants.

Which option does the Planning Commission prefer?

5. The proposed FBC allows for residential use on the first floor if adequate story height is met to ensure the potential for re-use of the space as commercial if desired in the future. The existing code does not allow ground floor residential uses (see Section 34-620 and 34-640 in the proposed code sections document). Staff suggests consideration of amending the existing code to allow for ground floor residential with a minimum story height (fifteen (15) feet is specified in the proposed FBC). The Planning Commission may wish to consider addressing this issue City-wide during the code audit. The Planning Commission may wish to consider amended the sections noted above to the following:

The following uses are prohibited within a building that fronts on a primary street, within any ground floor areas adjacent to such frontage:

- (1) *Dwelling units and guest rooms (residential and transient occupancies).*
- (2) *Parking garages, except ingress/egress.*

If a building has frontage on more than one primary street, the ground floor area adjacent to one (1) primary street may contain dwelling units or guest rooms, but not on West Main Street.

Which option does the Planning Commission prefer?

6. The requirements for bicycle parking suggested in the proposed FBC and included in proposed draft code sections specifies percentages of bicycle parking that should be short-term or long-term (example: eighty (80) percent short-term and twenty (20) percent long-term). However, the minimum required number of spaces is two (2) for most uses. In the circumstance that less than five (5) spaces are required, it is not possible to meet the percentage designations for short-term and long-term, as less than one (1) space would need to be designated as long-term. Staff suggested consideration of two options:
 - a. Include a statement in the code authorizing the Director of Neighborhood Development Services to determine appropriate percentages of short-term and long-term spaces in instances where less than five (5) spaces are required.
 - b. Include a statement in the code indicating that where less than five (5) spaces are required, all spaces must be short-term.

Which option does the Planning Commission prefer?

7. The proposed text regarding required bicycle parking does not include “bike closets” in individual residential units to contributing to the bicycle parking requirements. The Planning Commission may wish to include such storage systems in the code as allowable in meeting the bicycle parking requirements. However, such storage systems may be appropriated by residents for other uses and not provide the desired bicycle parking opportunity.

Which option does the Planning Commission prefer?

8. The reorientation of the zoning districts from north-south to east-west requires changes to the Use Matrix in Section 34-796. The existing West Main Street South (WMS) allows more height than West Main Street North (WMN), as the proposed West Main Street West (WMW) allows more height than West Main Street East (WME). Staff proposes uses that are currently found in WMS but not WMN be allowed in WMW but not WME. These uses are noted in blue in the proposed Use Matrix document. Alternatively, the Planning Commission may wish to further study existing allowed uses and make more substantial changes to the Use Matrix.

Which option does the Planning Commission prefer?

9. The proposed FBC did not suggest changes to the City’s regulations on appurtenances, found in Section 34-1101. Many members of the public and various review boards have expressed

frustration with the existing code's ambiguity. Staff suggests the Planning Commission consider addressing the issue concurrently with consideration of the proposed code changes to the West Main Street districts. Planning Commission may wish to discuss removing Section 110-1(c), which rooftop allows appurtenances to include habitable space:

No rooftop appurtenance shall: (i) itself measure more than sixteen (16) feet in height above the building, or (ii) cover more than twenty-five (25) percent of the roof area of a building. A roof-top appurtenance may contain useable floor area, but such area may only be used for or as an accessory to a residential or commercial use allowed within the applicable zoning district.

Alternatively, the Planning Commission may wish to consider addressing this issue City-wide during the code audit.

Which option does the Planning Commission prefer?

10. A new definition (under Section 34-1200) has been proposed for *build-to-zone*, as the term does not exist in the current code. Planning Commission may choose to edit the definition, or alternatively remove the requirements for build-to-zones found in the proposed Sections 34-618(a)(1), 34-638(a)(2), 34-638(a)(1), and 34-638(a)(2).

Which option does the Planning Commission prefer?

11. The proposed new zoning map provided by the consultants along with the FBC does not simply re-label the existing lots found within the West Main Street North and West Main Street South zoning districts. Several lots are moved from adjacent zoning districts to be included in the new West Main Street West district, and lots currently found in the West Main Street South district were not included in the new West Main Street zoning categories. Staff has created a new map that follows the same overall boundaries of the existing West Main Street districts, but redraws the dividing line between the two categories. Alternatively, the Planning Commission may wish to pursue the boundaries provided by the consultant. If so, additional consideration of affected parcels is required. Both maps are provided in this packet of information.

Which option does the Planning Commission prefer?

WEST MAIN STREET PROPOSED CODE CHANGES

July 28th, 2015 Planning Commission Work Session

Black text: Proposed changes

Grey text: Existing code

CHAPTER 34

ARTICLE VI

DIVISION 1. – GENERAL

Sec. 34-541. - Mixed use districts—Intent and description.

(4) *West Main Street West Corridor*. The land use and lots on West Main Street west of the railroad bridge is generally larger in size than those east of the bridge. The West Main West district is established to provide the opportunity for large-scale redevelopment with respect to established patterns of commercial and residential development along West Main Street and neighborhoods in close proximity. Within this district, one of the primary goals is to provide a walkable, mixed use “main street” setting that encourages vibrant pedestrian activity. The following streets shall have the designations indicated:

- (a) Where only one street abuts a lot, that street is considered the primary street.
- (b) Where more than one street abuts a lot, the following are considered primary streets:
 - (1) West Main Street
 - (2) Roosevelt Brown Boulevard
 - (3) Jefferson Park Avenue
 - (4) Wertland Street
 - (5) 10th Street NW
- (c) Where a lot with multiple street frontages on the primary streets listed in section (b) exists, each frontage is considered a primary street.

(5) *West Main Street East Corridor*. The land use and lots on West Main Street east of the railroad bridge are smaller and older than those west of the bridge, containing existing buildings (many of them historic in character) that have been renovated to accommodate modern commercial uses. Established buildings are located in close proximity to the street on

which they front, and one of the primary goals of this district is to provide a walkable, mixed use “main street” setting that encourages vibrant pedestrian activity. Within the West Main Street East district, the following streets shall have the designations indicated:

- (a) Where only one street abuts a lot, that street is considered the primary street.
- (b) Where more than one street abuts a lot, the following are considered primary streets:
 - (1) West Main Street
 - (2) Commerce Street
 - (3) South Street
 - (4) Ridge Street
 - (5) 7th Street SW
 - (6) 4th Street NW
- (c) Where a lot with multiple street frontages on the primary streets listed in section (b) exists, each frontage is considered a primary street.

DIVISION 5. – REGULATIONS – WEST MAIN STREET WEST (“WMW”)

Sec. 34-617. – Height regulations.

The height regulations shall apply to buildings and structures within the West Main Street West Corridor district:

- (1) Minimum height: 2 stories
- (2) Maximum height: 6 stories

Sec. 34-618. – Streetwall regulations.

(a) Setbacks shall be required, as follows:

- (1) *Primary street frontage:* Ten (10) feet minimum; twenty (20) feet maximum. At least eighty (80) percent of the building façade width of a building must be in the build-to zone adjacent to a primary street.
- (2) *Linking street frontage:* Five (5) feet minimum; twelve (12) feet maximum. At least forty (40) percent of the building façade width of a building must be in the build-to zone adjacent to a primary street.
- (3) *Side and rear setback, adjacent to any low density residential district:* Twenty (20) feet, minimum.
- (4) *Side and rear setback, adjacent to any other zoning district:* None required.

(b) Stepback requirement.

The maximum height of the streetwall of any building or structure shall be forty (40) feet. At the top of the streetwall height, there shall be a minimum stepback of ten (10) feet.

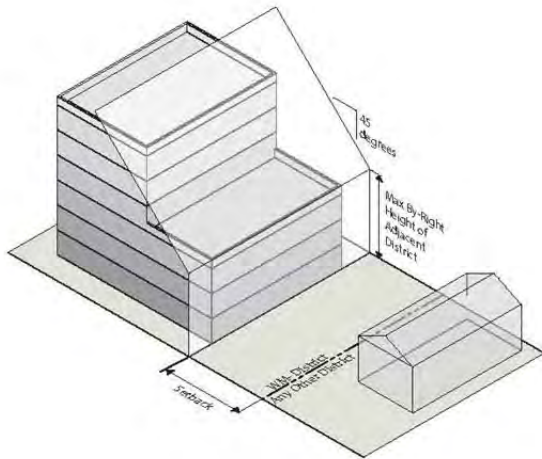
(c) Building width requirement.

Any building over two hundred (200) feet wide must be broken down to read as a series of buildings no wider than two hundred (200) feet each up to the point of stepback.

Sec. 34-619. – Bulk plane and buffer.

(a) Bulk plane.

- (1) To promote building massing compatible with adjacent districts, a bulk plane shall apply where a lot in the West Main Street West district abuts any other zoning district. No building may extend into a 45 degree angular plane projecting above the lot measured at the interior edge of any required setback, starting at a height equal to the maximum allowed height in the adjacent zoning district.
- (2) The bulk plane ends at each lot line adjacent to a street right-of-way



(b) Buffer.

Along the frontage with any low density residential district, side and rear buffers shall be required, ten (10) feet, minimum, consisting of an S-1 type buffer (refer to [section 34-871](#)).

Sec. 34-620. - Mixed-use developments—Additional regulations.

No ground floor residential uses or parking garage, other than ingress and egress to the garage, may front on a primary street, unless a building fronts on more than one (1) primary street, in

which case ground floor residential uses may front on one (1) primary street. Under no circumstances, however, shall any ground floor residential uses front on West Main Street.

Sec. 34-621. - Density.

Residential density shall not exceed forty-three (43) DUA; however, up to two hundred (200) DUA may be allowed by special use permit.

Sec. 34-622. - Additional regulations.

Developments that occupy an entire city block shall provide courtyards and plazas accessible from adjacent public rights-of-way.

Sec. 34-623. – Parking requirements adjustment.

Article VIII, Division 3, Off-Street Parking and Loading, applies, except that:

- (1) Parking lot buffers are required only along the edge(s) of a low density district.
- (2) No parking is required for any retail use having less than 5,000 square feet in floor area.

Sec. 34-624. – Bicycle parking requirements.

Bicycle parking spaces shall be required for new buildings and developments, the addition of new enclosed floor area to an existing building, and for any change in use of any building.

(a) Required bicycle spaces.

Use	Spaces Required	Short-Term/Long-Term
Residential	0.5 per unit	80%/20%
Public/Institutional	1 per 5,000 SF, 2 min	90%/10%
Food and drink service	1 per 2,500 SF, 2 min	80%/20%
Lodging	0.5 per guest room	80%/20%
All other commercial and industrial uses	1 per 2,500 SF, 2 min	80%/20%

(b) Location of bicycle parking.

- (1) Bicycle parking spaces must be located on paved or pervious, dust-free surface with a slope no greater than 3%. Surfaces cannot be gravel, landscape stone or wood chips.
- (2) Bicycle parking spaces must be a minimum of 2 feet by 6 feet. There must be an access aisle a minimum of 3 feet in width.
- (3) Each required bicycle parking space must be accessible without moving another bicycle, and its placement must not result in a bicycle obstructing a required walkway.
- (4) Up to 25% of bicycle parking may be structured parking, vertical parking or wallmount parking, provided there is a 5-foot access aisle for wall mount parking.
- (5) All racks must accommodate cable locks and "U" locks, must permit the locking of the bicycle frame and one wheel to the rack, and must support a bicycle in a stable position.

(c) Short-term bicycle parking.

Required short term parking should be visible from nearby bikeways and conveniently located to the main building entrance, no further than 50 feet. Short-term bicycle parking must meet all other applicable design standards of the City.

(d) Long-term bicycle parking.

- (1) Required long-term bicycle parking spaces must be located in enclosed and secured or supervised areas providing protection from theft, vandalism and weather, and must be accessible to intended users.
- (2) Required long-term bicycle parking for residential uses cannot be located within dwelling units or within deck, patio areas or private storage areas accessory to dwelling units.
- (3) With permission of the Director of Neighborhood Development Services, long-term bicycle parking spaces for nonresidential uses may be located off-site within 300 feet of the site. The off-site parking distance is measured in walking distance from the nearest point of the remote parking area to the closest primary entrance of the use served.

Secs. 34-625—34-635. - Reserved.

DIVISION 6. – REGULATIONS – WEST MAIN STREET EAST (“WME”)

Sec. 34-637. – Height regulations.

The height regulations shall apply to buildings and structures within the West Main Street East Corridor district:

- (1) Minimum height: 2 stories
- (2) Maximum height: 4 stories

Sec. 34-638. – Streetwall regulations.

(a) Setbacks shall be required, as follows:

- (1) *Primary street frontage*: Ten (10) feet minimum; twenty (20) feet maximum. At least eighty (80) percent of the building façade width of a building must be in the build-to zone adjacent to a primary street.
- (2) *Linking street frontage*: Five (5) feet minimum; twelve (12) feet maximum. At least forty (40) percent of the building façade width of a building must be in the build-to zone adjacent to a primary street.
- (3) *Side and rear setback, adjacent to any low density residential district*: Twenty (20) feet, minimum.
- (4) *Side and rear setback, adjacent to any other zoning district*: None required.

(b) Stepback requirement.

The maximum height of the streetwall of any building or structure shall be forty (40) feet. At the top of the streetwall height, there shall be a minimum stepback of ten (10) feet.

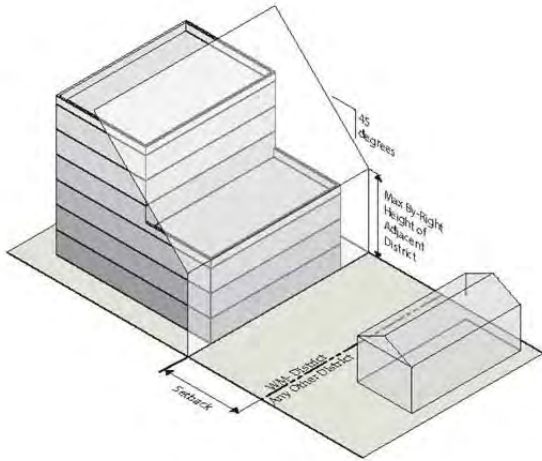
(c) Building width requirement.

Any building over two hundred (200) feet wide must be broken down to read as a series of buildings no wider than two hundred (200) feet each up to the point of stepback.

Sec. 34-639. – Bulk plane and buffer.

(a) Bulk plane.

- (1) To promote building massing compatible with adjacent districts, a bulk plane shall apply where a lot in the West Main Street East district abuts any other zoning district. No building may extend into a 45 degree angular plane projecting above the lot measured at the interior edge of any required setback, starting at a height equal to the maximum allowed height in the adjacent zoning district.
- (2) The bulk plane ends at each lot line adjacent to a street right-of-way.



(b) Buffer.

Along the frontage with any low density residential district, side and rear buffers shall be required, ten (10) feet, minimum, consisting of an S-1 type buffer (refer to section 34-871).

Sec. 34-640. - Mixed-use developments—Additional regulations.

No ground floor residential uses or parking garage, other than ingress and egress to the garage, may front on a primary street, unless a building fronts on more than one (1) primary street, in which case ground floor residential uses may front on one (1) primary street. Under no circumstances, however, shall any ground floor residential uses front on West Main Street.

Sec. 34-641. - Density.

Residential density shall not exceed forty-three (43) DUA; however, up to two hundred (200) DUA may be allowed by special use permit.

Sec. 34-642. - Additional regulations.

Developments that occupy an entire city block shall provide courtyards and plazas accessible from adjacent public rights-of-way.

Sec. 34-643. – Parking requirements adjustment.

Article VIII, Division 3, Off-Street Parking and Loading, applies, except that:

- (1) Parking lot buffers are required only along the edge(s) of a low density district.
- (2) No parking is required for any retail use having less than 5,000 square feet in floor area.

Sec. 34-644. – Bicycle parking requirements.

Bicycle parking spaces shall be required for new buildings and developments, the addition of new enclosed floor area to an existing building, and for any change in use of any building.

(a) Required bicycle spaces.

Use	Spaces Required	Short-Term/Long-Term
Residential	0.5 per unit	80%/20%
Public/Institutional	1 per 5,000 SF, 2 min	90%/10%
Food and drink service	1 per 2,500 SF, 2 min	80%/20%
Lodging	0.5 per guest room	80%/20%
All other commercial and industrial uses	1 per 2,500 SF, 2 min	80%/20%

(b) Location of bicycle parking.

- (1) Bicycle parking spaces must be located on paved or pervious, dust-free surface with a slope no greater than 3%. Surfaces cannot be gravel, landscape stone or wood chips.
- (2) Bicycle parking spaces must be a minimum of 2 feet by 6 feet. There must be an access aisle a minimum of 3 feet in width.
- (3) Each required bicycle parking space must be accessible without moving another bicycle, and its placement must not result in a bicycle obstructing a required walkway.
- (4) Up to 25% of bicycle parking may be structured parking, vertical parking or wallmount parking, provided there is a 5-foot access aisle for wall mount parking.
- (5) All racks must accommodate cable locks and "U" locks, must permit the locking of the bicycle frame and one wheel to the rack, and must support a bicycle in a stable position.

(c) Short-term bicycle parking.

Required short term parking should be visible from nearby bikeways and conveniently located to the main building entrance, no further than 50 feet. Short-term bicycle parking must meet all other applicable design standards of the City.

(d) Long-term bicycle parking.

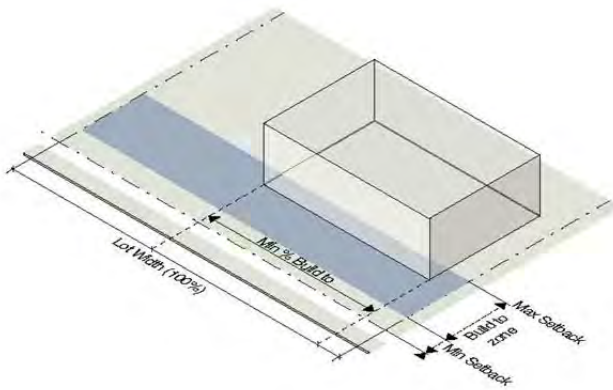
- (1) Required long-term bicycle parking spaces must be located in enclosed and secured or supervised areas providing protection from theft, vandalism and weather, and must be accessible to intended users.
- (2) Required long-term bicycle parking for residential uses cannot be located within dwelling units or within deck, patio areas or private storage areas accessory to dwelling units.
- (3) With permission of the Director of Neighborhood Development Services, long-term bicycle parking spaces for nonresidential uses may be located off-site within 300 feet of the site. The off-site parking distance is measured in walking distance from the nearest point of the remote parking area to the closest primary entrance of the use served.

Secs. 34-645—34-655. - Reserved.

ARTICLE X

Sec. 34-1200. – Definitions.

Build-to-zone: The area between the minimum and maximum allowable setbacks along a street frontage. A building façade may be required to maintain a minimum percentage in the build-to-zone, measured based on the width of the building divided by the width of the lot. Minor deviations such as recessed entries, recessed balconies, and architectural features do not count against calculations of percentage requirements.



WEST MAIN STREET PROPOSED CODE CHANGES

July 28th, 2015 Planning Commission Work Session

Areas of Proposed Changes

	Use Types with Existing Differences Between WMN and WMS (Decision Points)
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Sec. 34-796. - Use matrix—Mixed use corridor districts.

The uses and residential densities allowed within the city's mixed use corridor districts are those identified within the matrix following below. (For a list of each of the city's zoning districts and their abbreviations, see section 34-216).

A = Ancillary use	DUA = dwelling units per acre	P = provisional use permit
B = by-right use	GFA = gross floor area	S = special use permit
CR = commercial/residential	MFD = multifamily development	T = temporary use permit
M = mixed use development	M/S = mixed use or special use permit	A/S = Ancillary or special use permit

Use Types	Existing			
	WMN	WMS	WMW	WME
RESIDENTIAL AND RELATED USES				
Accessory apartment, internal				
Accessory apartment, external				

	WMN	WMS	WMW	WME
Accessory buildings, structures and uses (residential)	B	B	B	B
Adult assisted living				
1—8 residents	B	B	B	B
Greater than 8 residents				
Adult day care				
Amateur radio antennas, to a height of 75 ft.				
Bed-and-breakfasts:				
Homestay	B	B	B	B
B & B	B	B	B	B
Inn	B	B	B	B
Boarding: fraternity and sorority house				
Boarding house (rooming house)				
Convent/monastery	B	B	B	B
Criminal justice facility				
Dwellings:				
Multifamily	M	M	M	M
Single-family attached	B	B	B	B
Single-family detached	B	B	B	B

	WMN	WMS	WMW	WME
Townhouse				
Two-family				
Family day home				
1—5 children	B	B	B	B
6—12 children				
Home occupation	P	P	P	P
Manufactured home parks				
Night watchman's dwelling unit, accessory to industrial use				
Nursing homes				
Occupancy, residential				
3 unrelated persons				
4 unrelated persons	B	B	B	B
Residential treatment facility				
1—8 residents	B	B	B	B
8+ residents	S	S	S	S
Shelter care facility	S	S	S	S
Single room occupancy facility	S	S	S	S
Temporary family health care structure	T	T	T	T
NON-RESIDENTIAL: GENERAL AND MISC. COMMERCIAL				

	WMN	WMS	WMW	WME
Access to adjacent multifamily, commercial, industrial or mixed-use development or use				
Accessory buildings, structures and uses	B	B	B	B
Amusement center	S	S	S	S
Amusement enterprises (circuses, carnivals, etc.)				
Amusement park (putt-putt golf; skateboard parks, etc.)				
Animal boarding/grooming/kennels:				
With outside runs or pens				
Without outside runs or pens				
Animal shelter				
Art gallery:				
GFA 4,000 SF or less	B	B	B	B
GFA up to 10,000 SF	B	B	B	B
Art studio, GFA 4,000 SF or less	B	B	B	B
Art workshop	B	B	B	B
Assembly (indoor)				
Arena, stadium (enclosed)				
Auditoriums, theaters				
Maximum capacity less than 300 persons	B	B	B	B

	WMN	WMS	WMW	WME
Maximum capacity greater than or equal to 300 persons	S	S	S	S
Houses of worship	B	B	B	B
Assembly (outdoor)				
Amphitheater	S	S	S	S
Arena, stadium (open)				
Temporary (outdoor church services, etc.)	T	T	T	T
Assembly plant, handcraft				
Assembly plant				
Automobile uses:				
Gas station				
Parts and equipment sales		B	B	
Rental/leasing				
Repair/servicing business				
Sales				
Tire sales and recapping				
Bakery, wholesale				
GFA 4,000 SF or less	B	B	B	B
GFA up to 10,000 SF				
Banks/ financial institutions	B	B	B	B

	WMN	WMS	WMW	WME
Bowling alleys				
Car wash				
Catering business	B	B	B	B
Cemetery				
Clinics:				
Health clinic (no GFA limit)				
Health clinic (up to 10,000 SF, GFA)	B	B	B	B
Health clinic (up to 4,000 SF, GFA)	B	B	B	B
Public health clinic	B	B	B	B
Veterinary (with outside pens/runs)				
Veterinary (without outside pens/runs)				
Clubs, private	S	S	S	S
Communications facilities and towers:				
Antennae or microcells mounted on existing towers established prior to 02/20/01	B	B	B	B
Attached facilities utilizing utility poles or other electric transmission facilities as the attachment structure	B	B	B	B
Attached facilities not visible from any adjacent street or property	B	B	B	B
Attached facilities visible from an adjacent street or property	S	S	S	S

	WMN	WMS	WMW	WME
Alternative tower support structures				
Monopole tower support structures				
Guyed tower support structures				
Lattice tower support structures				
Self-supporting tower support structures				
Contractor or tradesman's shop, general				
Crematorium (independent of funeral home)				
Data center >4,000		S	S	
<4,000	B	B	B	B
Daycare facility	B	B	B	B
Dry cleaning establishments	B	B	B	B
Educational facilities (non-residential)				
Elementary	B	B	B	B
High schools	B	B	B	B
Colleges and universities				
Artistic instruction, up to 4,000 SF, GFA	B	B	B	B
Artistic instruction, up to 10,000 SF, GFA		B	B	
Vocational, up to 4,000 SF, GFA				
Vocational, up to 10,000 SF, GFA				

	WMN	WMS	WMW	WME
Electronic gaming café				
Funeral home (without crematory)				
GFA 4,000 SF or less	B	B	B	B
GFA up to 10,000 SF	S	S	S	S
Funeral homes (with crematory)				
GFA 4,000 SF or less				
GFA up to 10,000 SF				
Golf course				
Golf driving range				
Helipad				
Hospital	S	S	S	S
Hotels/motels:				
Up to 100 guest rooms	B	B	B	B
100+ guest rooms	S	B	B	S
Laundromats				
Libraries	B	B	B	B
Manufactured home sales				
Microbrewery	B	B	B	B
Mobile food units	P	P	P	P

	WMN	WMS	WMW	WME
Movie theaters, cineplexes	S	S	S	S
Municipal/governmental offices, buildings, courts	B	B	B	B
Museums:				
Up to 4,000 SF, GFA	B	B	B	B
Up to 10,000 SF, GFA	S	B	B	S
Music hall	B	B	B	B
Offices:				
Business and professional	B	B	B	B
Medical	B	B	B	B
Philanthropic institutions/agencies	B	B	B	B
Property management (ancillary to MFD)	A	A	A	A
Other offices (non-specified)	B	B	B	B
Outdoor storage, accessory				
Parking:				
Parking garage	A/S	A/S	A/S	A/S
Surface parking lot (19 or less spaces)	B	B	B	B
Surface parking lot (more than 20 spaces)	A	A	A	A
Temporary parking facilities				
Photography studio	B	B	B	B

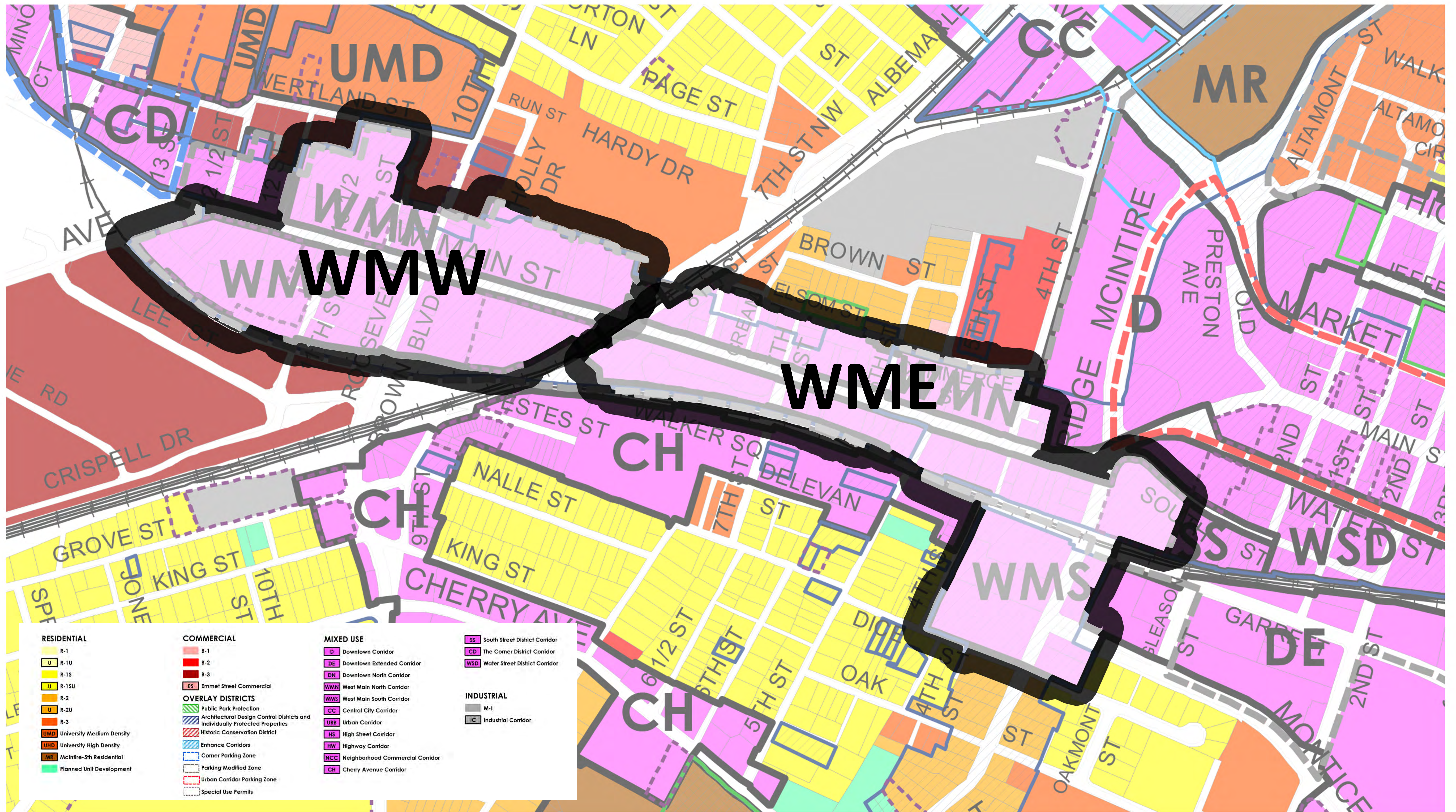
	WMN	WMS	WMW	WME
Photographic processing; blueprinting				
Radio/television broadcast stations	B	B	B	B
Recreational facilities:				
Indoor: health/sports clubs; tennis club; swimming club; yoga studios; dance studios, skating rinks, recreation centers, etc. (on City-owned, City School Board-owned, or other public property)	B	B	B	B
Indoor: health/sports clubs; tennis club; swimming club; yoga studios; dance studios, skating rinks, recreation centers, etc. (on private property)				
GFA 4,000 SF or less	B	B	B	B
GFA (4,001—10,000 SF)	S	B	B	S
GFA more than 10,000 SF	S	B	B	S
Outdoor: Parks, playgrounds, ball fields and ball courts, swimming pools, picnic shelters, etc. (city-owned), and related concession stands	B	B	B	B
Outdoor: Parks, playgrounds, ball fields and ball courts, swimming pools, picnic shelters, etc. (private)	S	S	S	S
Restaurants:				
All night	S	S	S	S
Drive-through windows				
Fast food	B	B	B	B
Full service	B	B	B	B

	WMN	WMS	WMW	WME
24-hour				
Towing service, automobile				
Technology-based businesses	B	B	B	B
Taxi stand	S	S	S	S
Transit facility	B	B	B	B
Utility facilities	S	S	S	S
Utility lines	B	B	B	B
NON-RESIDENTIAL USES: RETAIL				
Accessory buildings, structures and uses	B	B	B	B
Consumer service businesses:				
Up to 4,000 SF, GFA	B	B	B	B
Up to 10,000 SF, GFA	B	B	B	B
10,001+ GFA	S	S	S	S
Farmer's market	S	S	S	S
Greenhouses/nurseries				
Grocery stores:				
Convenience	B	B	B	B
General, up to 10,000 SF, GFA	S	B	B	S
General, 10,001+ SF, GFA	S	B	B	S

	WMN	WMS	WMW	WME
Home improvement center				
Pharmacies:				
1—1,700 SF, GFA	B	B	B	B
1,701—4,000 SF, GFA	B	B	B	B
4,001+ SF, GFA	B	B	B	B
Shopping centers	S	S	S	S
Shopping malls	S	S	S	S
Temporary sales, outdoor (flea markets, craft fairs, promotional sales, etc.)				
Other retail stores (non-specified):				
Up to 4,000 SF, GFA	B	B	B	B
Up to 20,000 SF GFA	S	B	B	S
20,000+ SF, GFA		S	S	
NON-RESIDENTIAL: INDUSTRIAL				
Accessory buildings, structures and uses				
Assembly, industrial				
Beverage or food processing, packaging and bottling plants				
Brewery and bottling facility				
Compounding of cosmetics, toiletries, drugs and pharmaceutical products				

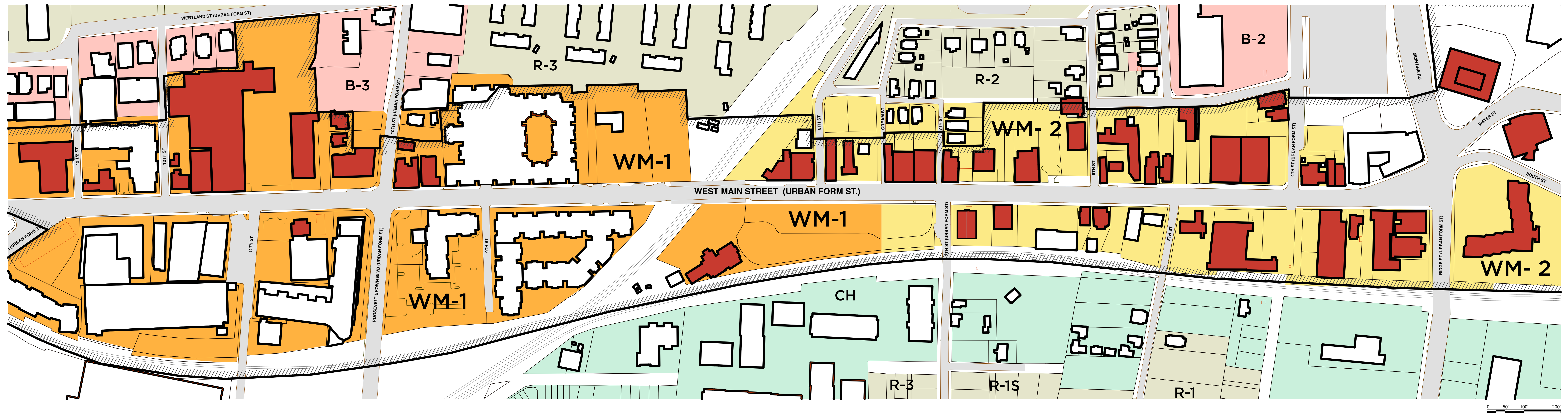
	WMN	WMS	WMW	WME
Construction storage yard				
Contractor or tradesman shop (HAZMAT)				
Frozen food lockers				
Greenhouse/nursery (wholesale)				
Industrial equipment: service and repair				
Janitorial service company				
Kennels				
Laboratory, medical >4,000 sq. ft.	B	B	B	B
<4,000 sq. ft.	B	B	B	B
Laboratory, pharmaceutical >4,000 sq. ft.	S	S	S	S
<4,000 sq. ft.	B	B	B	B
Landscape service company				
Laundries				
Manufactured home sales				
Manufacturing, light				
Moving companies				
Printing/publishing facility	S	S	S	S
Open storage yard				
Outdoor storage, accessory to industrial use				

	WMN	WMS	WMW	WME
Research and testing laboratories	B	B	B	B
Self-storage companies				
Warehouses				
Welding or machine shop				
Wholesale establishments				



PROPOSED WEST MAIN ZONING DISTRICTS

28 JULY 2015



ZONING MAP KEY

- ADC DISTRICT
- ECONOMIC ANALYSIS LOT(S)
- EXISTING STRUCTURES**
- CONTRIBUTING
- OTHER

- PROPOSED DISTRICTS**
- WM-1 WEST MAIN 1
- WM-2 WEST MAIN 2

- ADJACENT DISTRICTS**
- MIXED USE
- RESIDENTIAL
- COMMERCIAL
- INDUSTRIAL

WEST MAIN ST

CHARLOTTESVILLE

Nelson\Nygaard Consulting Associates
 Bushman Dreyfus Architects
 Code Studio
 RCLCO
 Schulze + Grasso
 Sadler & Whitehead
 Timmons Group
 Low Impact Development Center

08.05.14

ZONING OF WEST MAIN ST.



**CITY OF CHARLOTTESVILLE
NEIGHBORHOOD DEVELOPMENT SERVICES
MEMO**

To: City of Charlottesville Planning Commission
From: Brian Haluska, Principal Planner
CC: Missy Creasy, Interim Director
Date: July 30, 2015
Re: Development Review Process Policy

Introduction

At their meeting on February 2, 2015; City Council initiated a zoning text amendment to modify the way in which the City reviews development applications. Specifically, the proposed changes would not immediately refer complete applications for development (rezoning requests, special use permits, site plans and subdivision plats) to the Planning Commission upon receipt, but would rather give the Director of Neighborhood Development Services and City Council the ability to hold off on referring the item to the Commission. The additional time in the process would be used for potential work sessions on the project with the Planning Commission, a mandatory community meeting arranged by the applicant, and staff review that could result in a request for additional information from the applicant in order to better explain their request.

At their May 12, 2015 meeting, the Planning Commission recommended changes to the development review process to City Council. At their meeting on June 1, 2015, City Council reviewed the proposed changes and expressed concern about the provision that would permit the Director of NDS to waive the requirement. Council directed staff to draft a document that would provide further guidance to the Director of NDS and staff about when it may be appropriate to waive the public meeting requirement.

Public Process

The Planning Commission held a joint public hearing with City Council on this matter at their meeting on May 12, 2015. Two members of the public spoke at the meeting. The first speaker expressed concern with the amount of staff time necessary to implement the changes, while recommending that staff be responsible for arranging the public meetings. The speaker also questioned the City's legal authority to require a meeting with neighborhood on by-right projects.

The second speaker noted that the site plan conferences the City currently hosts are held during typical work hours, making attendance difficult for some citizens. The speaker also noted that the

additional meetings, especially for site plans, would require many more night meetings for staff to attend.

Policy Summary

Staff divided the applications subject to the new public meeting requirement into three categories:

- The first category is for applications where the Director would not waive the public meeting requirement under any circumstances.
- The second category is for applications where the assumption is that the public meeting would be held, unless the Director specifically decides to waive the requirement
- The third category is for applications where the assumption is that the public meeting would NOT be held, unless the Director specifically directs staff to hold a public meeting.

When referring to the draft policy, the divisions within the site plan and subdivision applications in terms of lots created, parking spaces, or square footage of additions are not distinctions found in the City zoning ordinance, but rather City staff's attempt at quantifying a dividing line between applications that would potentially benefit from the public meeting process, versus applications where meetings would be cumbersome.

Questions for Discussion

Staff intends to ask the following questions during the work session:

1. Is the policy in keeping with the Commission's recommendation to Council on this matter?
2. Are the categories/policy easy for the public to understand?

Supporting Documents

- Draft Application Process Waiver Policy

Applications for which a meeting would be called in all situations:

1. Rezoning
2. Special Use Permits
 - Requests for additional height
 - Requests for density greater than by-right density
3. Preliminary or Final Site Plans
 - Greater than 6 residential units proposed
 - Construction of a mixed-use, commercial or industrial structure greater than 5,000 sq. ft.
 - Expansion of a mixed-use, commercial or industrial structure by more than 5,000 sq. ft. GFA.
 - Proposed addition of more than 10 parking spaces.
4. Major Subdivisions

Applications for which a meeting would be called unless waived by the Director of NDS:

1. Rezoning
 - Modifications to an existing Planned Unit Development
2. Special Use Permits
 - Requests for a use in an existing building
 - Alterations to an existing SUP
3. Preliminary or Final Site Plans
 - Greater than 2 residential units proposed
 - Construction of a mixed-use, commercial or industrial structure greater than 2,000 sq. ft.
 - Expansion of a mixed-use, commercial or industrial structure by more than 2,000 sq. ft. GFA.
 - Proposed addition of more than 5 parking spaces.
4. Minor Subdivisions
 - Creating more than 2 new lots

Applications for which a meeting would NOT be called unless required by the Director of NDS:

1. Preliminary or Final Site Plans
 - Construction of a mixed-use, commercial or industrial structure less than 2,000 sq. ft.
 - Expansion of a mixed-use, commercial or industrial structure by less than 2,000 sq. ft. GFA.
 - Proposed addition of less than 5 parking spaces.
2. Minor Subdivisions
 - Creating less than 2 new lots
 - Boundary line adjustments
 - Vacation of interior property lines
3. Site Plan Amendments - "A minor modification is one (1) that, in the opinion of the director, will not substantially alter the terms of the original approval." (Sec. 34-826)